

THE LAB PARKING STRUCTURE

USE PERMIT - RESUBMITTAL
DECEMBER 22, 2021

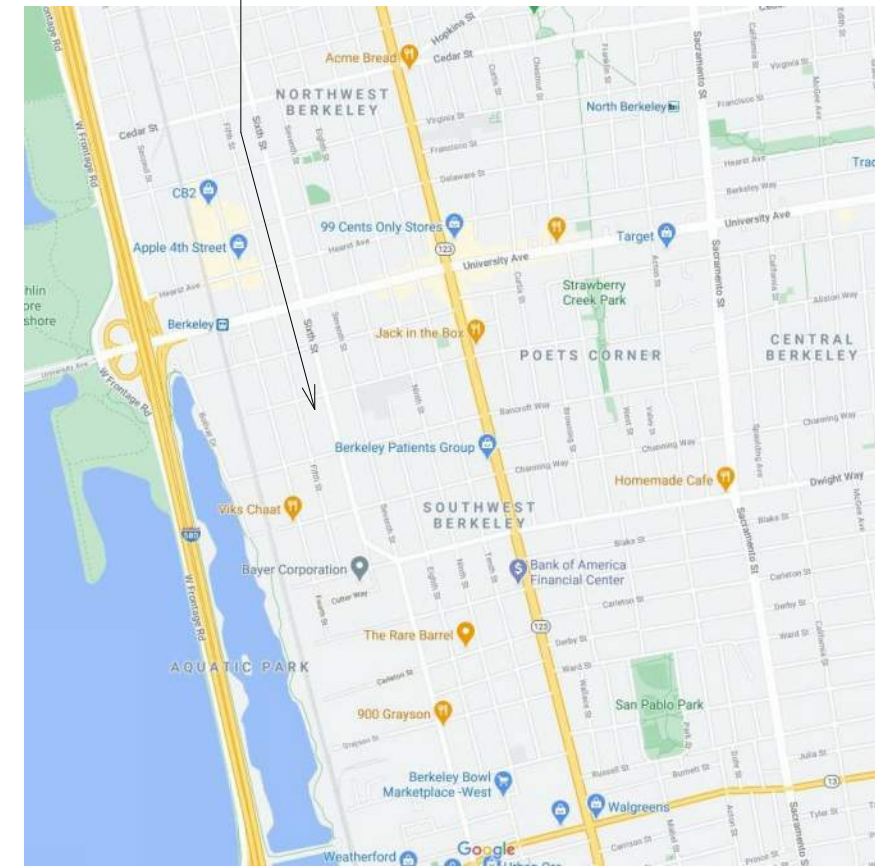


TheLAB Parking Structure 2212/2216 5TH ST &
2213/2221 4TH ST.
BERKELEY, CA 94510



INTERNATIONAL PARKING DESIGN, INC.
560 14TH STREET,
SUITE 300
OAKLAND CA 94612
T. 510.473.0300

PROJECT SITE



LOCATION MAP SCALE: N.T.S.

PROJECT SITE



SITE CONTEXT SCALE: N.T.S.

Project Description

The purpose of the following report is to identify and develop a 415 space parking structure in Southwest Berkeley, California. The site is located east of Fourth Street and west of Fifth Street between Allston Way and Bancroft Way. The proposed Parking Structure will be primarily used for employee parking for the adjacent office and lab uses. The structure will span the entire width of the existing block between Fourth and Fifth Street and occurs roughly midblock between Allston and Bancroft Ways. Vehicular ingress and egress aisles are anticipated to be positioned at the east and west ends of the structure with curb cuts on Fourth and Fifth Street. Positioning of vehicular access at the north end of the structure on Fourth Street allows for landscaped buffers and an uninterrupted pedestrian access to the office and lab buildings for which the garage serves. In addition to passenger vehicles, the project provides a single bay loading facility which will be accessed off of Fourth Street. A private use landscaped parklet will also be provided north of the proposed parking structure which will be used for storm water treatment as well as be a secured outdoor area for the adjacent office and lab uses.

With a projected capacity of 415 vehicular spaces, the garage will likely be 4-levels above grade on Fourth Street, approximately 45-feet in height and 3 levels above grade on Fifth Street, approximately 35-feet in height. Vertical circulation cores will be located on both frontages of the building. With the western core on Fourth Street providing a single passenger elevator. The orientation of the parking structure will allow for adequate areas of the garage walls to be open, thus negating the need for mechanical ventilation as per the California Building Code exception for Open Parking Structures – a greener, more economical solution. Lighting of the structure will follow current California standards; which include the use of LED fixtures and occupancy sensors, reducing maintenance outlays and energy costs. The roof level could be made available to photovoltaic arrays, off-setting the structure's energy demands and approaching or attaining a Net-Zero Energy Project. Current code requirements mandate that only the infrastructure and service sizing for 6% electric vehicle charging stations be provided. Provisions include electrical service capacity for the future stations, and are included in the design. The Owner may elect to add the charging stations now, though it is not essential for code compliance. Landscaped stormwater filtration basins will be provided to filter rainwater on-site and alleviate run-off from inundating the municipal storm system. The parking levels and circulation cores will be differentiated and signed for simple way-finding for both drivers and pedestrians. The architectural treatment of the prominent façade elevations will be compatible with existing and proposed future developments adjacent to the site and be a welcome addition to what is becoming a center for lab, research and development and commercial office uses within the City of Berkeley. Architecture facade elements in the garage is provided as art form on elevation facing 4th & 5th street in an effort to meet the art allocation. See the Public Art Declaration

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PROJECT DATA	
PROJECT ADDRESS:	2212/2216 5TH ST & 2213/2221 4TH ST
ACCESSOR PARCEL NUMBER:	56-1958-6-4 / 56-1958-14-1 / 56-1958-4
TYPE OF CONSTRUCTION:	TYPE 2A ALLOWABLE AREA/FLOOR PER TABLE 406.5.4 50,000 S.F./10 TIERS ACTUAL AREA/LEVEL = 124,667 S.F./5 TIERS < 50,000 S.F. T/10 TIERS = O.K
ZONNING:	MUR/MULI
OCCUPANCY: (CBC 311.3)	S-2 OPEN PARKING GARAGE)CC 406.4 & 406.6 S-1 GENERAL STORAGE
NO. OF STORIES:	5 TIERS
FIRE PROTECTION:	DRY STANDPIPES IN S-1 & S-2 OCCUPANY
VENTILATION:	NATURALLY VENTILATED GROUND LEVEL TO THE ROOF
REQUIRED EXITS:	TWO (2) CBC SECTION 1019.11 TWO (2) PROVIDED = OK

PARKING SPACE & AREA SUMMARY																
AREA	ON-GRADE SLAB (S.F.)	ELEVATED SLAB (S.F.)	M	B	DESIGNATED PARKING						PARKING STALL TYPE				TOTAL	S.F./STALL
					CAV	EV-AMB	EV-V	EV-A	EV-CS	AS	VS	CS	FS	C		
NON-PARKING AREA:																
SERVICE AREA**	2,386	64														
CIRCULATION AREA**	331	450														
SUB TOTAL:	2,717	514														
TOTAL:	3,231															
PARKING AREA:																
GROUND LEVEL	27,009			3	1	1	1	9	6	2	23	36	82	329.38		
SECOND LEVEL	570	28,156		2				10			48	40	100	287.26		
THIRD LEVEL		29,965		2				10			55	40	107	280.05		
FOURTH LEVEL		29,965		2				10			55	40	107	280.05		
FIFTH (ROOF) LEVEL		5,771									9	10	19	303.74		
SUB TOTAL:	27,579	93,857	0	0	9	1	1	1	39	6	2	0	190	166	415	292.62
TOTAL:	121,436															
TOTAL PARKING AND NON-PARKING AREA:	30,296	94,371														
GROSS BLDG. AREA (S.F.):		124,667 S.F.														
DESIGN EFFICIENCY		292.62 SF/STALL														
PERCENT OF COMPACTS TO TOTAL:		40% (50% OF TOTAL PARKING ALLOWED)														
EVCS		10% (6% OF TOTAL PARKING REQUIRED)														
PERCENT OF DESIGNATED PARKING TO TOTAL:		12% (8% OF TOTAL PARKING REQUIRED)														
LEGEND:													PER BMC 5.106.5.3.3, 166 PARKING SPACES (40% OF TOTAL PARKING NUMBER) SHALL SUPPORT FUTURE EVSE.			
M - MOTORCYCLE																
B - BIKES																
CS - CAR SHARE																
CAV - COMBINATION OF LOW-EMITTING, FUEL EFFICIENT AND CARPOOL/VAN POOL VEHICLES																
EV-AMB - AMBULATORY ELECTRIC VEHICLE CHARGING STATION																
EV-V - VAN ACCESSIBLE ELECTRIC VEHICLE CHARGING STATION																
EV-A - REGULAR ACCESSIBLE ELECTRIC VEHICLE CHARGING STATION																
EVCS - ELECTRIC VEHICLE CHARGING STATION																
AS - REGULAR ACCESSIBLE STALL																
VS - VAN ACCESSIBLE STALL																
FS - FULL SIZE STALL																
C - COMPACT STALL																
* DOES NOT INCLUDE MOTORCYCLE AND BIKE SPACES																
** INCLUDES ELECT., MECH., SWEEPER, TRASH AND STORAGE AREAS																

12/22/2021

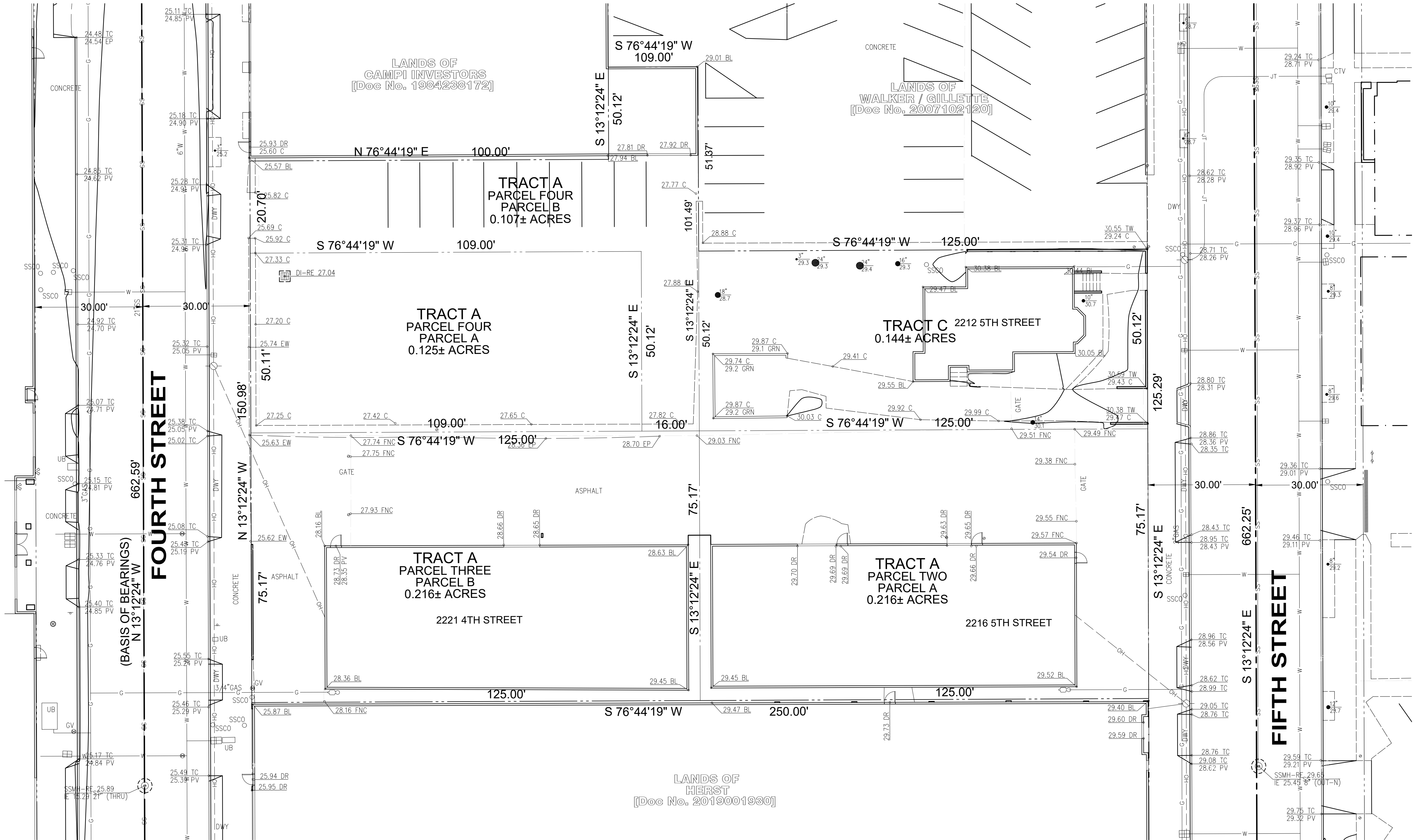
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TOPOGRAPHIC SURVEY C-1

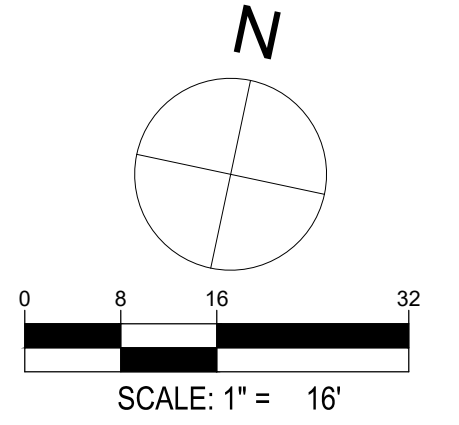


- NOTES**
- All distances shown hereon are in U.S. Survey feet and decimals thereof.
 - This survey was prepared from information furnished in a Preliminary Title Report, prepared by First American Title Insurance Company dated September 23, 2019, Order No. NCS-984188-SC. No liability is assumed for matters of record not stated in said Preliminary Title Report that may affect the boundary lines, exceptions, or easements affecting the property.
 - The types, locations, sizes and/or depths of existing underground utilities as shown on this topographic survey were obtained from sources of varying reliability. The contractor is cautioned that only actual excavation will reveal the types, extent, sizes, locations and depths of such underground utilities. (A reasonable effort has been made to locate and delineate all unknown underground utilities.) However, the surveyor can assume no responsibility for the completeness or accuracy of its delineation of such underground utilities which may be encountered, but which are not shown on these drawings.
 - A.P.N.: 056-1958-014-01 and 056-1958-006-04
 - Basis of Bearings:
The bearing of North 13°12'24" West on the monument line of 4th Street was taken as the Basis of all Bearings shown hereon.
 - Benchmark:
City of Berkeley Benchmark "B0462":
Found City monument located at the intersection of Allston Way and 4th Street.
Elevation: 24.43 feet (Vertcon) (Datum) NAVD 1988
Subtract 5.85 feet to match City of Berkeley Datum
Elevation: 18.58 feet (City of Berkeley Datum)
 - Flood Zone Note:
The subject property is shown on the Federal Emergency Management Agency Flood Insurance Rate Map, Community Panel Number 060004 0056 H, dated December 21, 2018, as being located in Flood Zone "X";
Areas determined to be outside the 0.2% annual chance flood.
Information was obtained from the FEMA website (www.fema.gov) on April 27, 2020.

LEGEND

PROPERTY LINE	BACK FLOW PREVENTER
ADJACENT PROPERTY LINE	UTILITY BOX (SIZE VARIES)
CENTERLINE	MONITORING WELL
MONUMENT LINE	SIGN
BUILDING LINE W/ DOOR	TREE W/ SIZE AND ELEVATION
BUILDING OVERHANG	SPOT ELEVATION
FOUND MONUMENT AS NOTED	CONTOUR
BOLLARD LIGHT	INDEX CONTOUR
LIGHT	CURB
STREET LIGHT	CURB & GUTTER
TRANSFORMER	CONCRETE
FIRE HYDRANT	FENCE
STORM DRAIN MANHOLE	RETAINING WALL
SANITARY SEWER MANHOLE	EDGE OF PAVEMENT
CLEAN OUT	SANITARY SEWER
GAS METER	STORM DRAIN
UTILITY POLE W/ GUY WIRE	WATER
VALVE	GAS
CATCH BASIN / DROP INLET	UNDERGROUND ELECTRIC
WATER METER	OVERHEAD
FIRE DEPARTMENT CONNECTION	JOINT TRENCH

1 TOPOGRAPHIC SURVEY
SCALE: 1/16" = 1'-0"



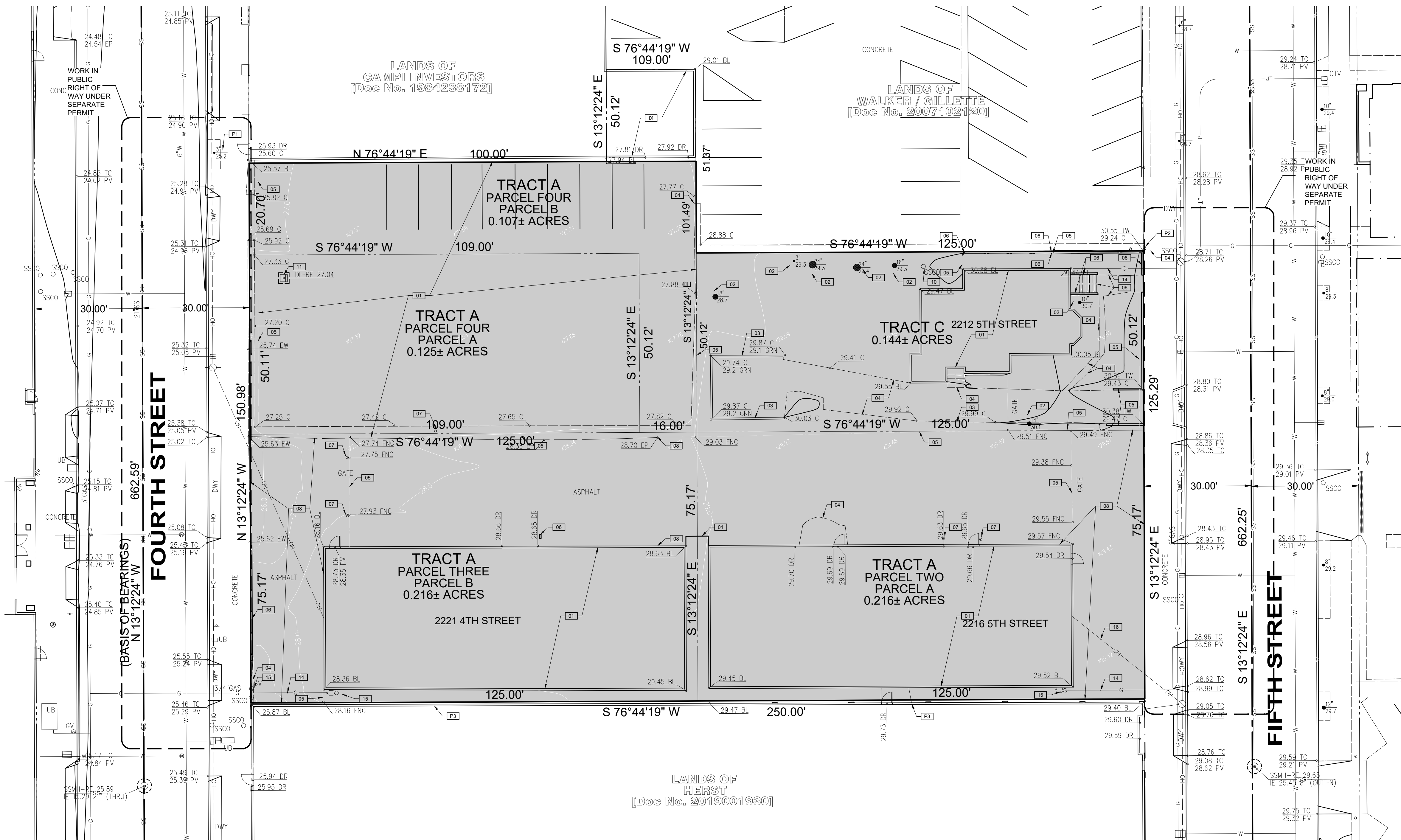
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DEMOLITION PLAN C-2



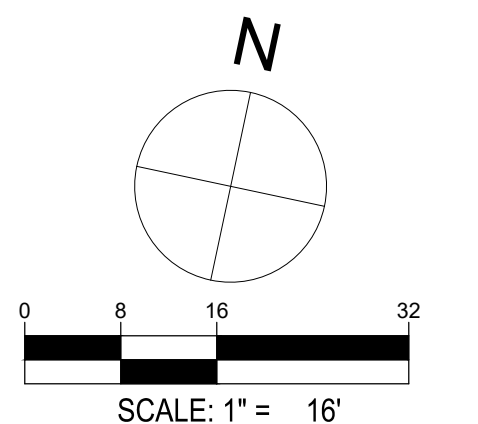
NOTES

P1	PROTECT TREE	13	DEMOLISH WATER LINE
P2	PROTECT BOLLARD	14	CAP NATURAL GAS LINE AT PROP LINE
P3	PROTECT BUILDING	15	GAS METER
P4	PROTECT OVERHEAD ELECTRIC	16	OVERHEAD ELECTRIC LINE
P5	PROTECT UTILITY POLE		
01	DEMOLISH BUILDING		
02	REMOVE TREE		
03	DEMOLISH CURB & GUTTER		
04	DEMOLISH CONCRETE / SIDEWALK		
05	DEMOLISH FENCE		
06	DEMOLISH WALL		
07	DEMOLISH BOLLARD		
08	DEMOLISH ASPHALT		
09	DEMOLISH UTILITY POLE		
10	DEMOLISH SANITARY CLEANOUT		
11	DEMOLISH STORM INLET		
12	DEMOLISH WATER METER / STRUCTURE		

LEGEND

PROPERTY LINE	BACK FLOW PREVENTER
ADJACENT PROPERTY LINE	UTILITY BOX (SIZE VARIES)
CENTERLINE	MONITORING WELL
MONUMENT LINE	SIGN
BUILDING LINE W/ DOOR	TREE W/ SIZE AND ELEVATION
BUILDING OVERHANG	SPOT ELEVATION
FOUND MONUMENT AS NOTED	CONTOUR
BOLLARD LIGHT	INDEX CONTOUR
LIGHT	CURB
STREET LIGHT	CURB & GUTTER
TRANSFORMER	CONCRETE
FIRE HYDRANT	FENCE
STORM DRAIN MANHOLE	RETAINING WALL
SANITARY SEWER MANHOLE	EDGE OF PAVEMENT
CLEAN OUT	SANITARY SEWER
GAS METER	STORM DRAIN
UTILITY POLE W/ GUY WIRE	WATER
VALVE	GAS
CATCH BASIN / DROP INLET	UNDERGROUND ELECTRIC
WATER METER	OVERHEAD
FIRE DEPARTMENT CONNECTION	JOINT TRENCH
LIMITS OF DEMOLITION, SEE KEYNOTES FOR DETAILS	

2 DEMOLITION PLAN
SCALE: 1/16" = 1'-0"



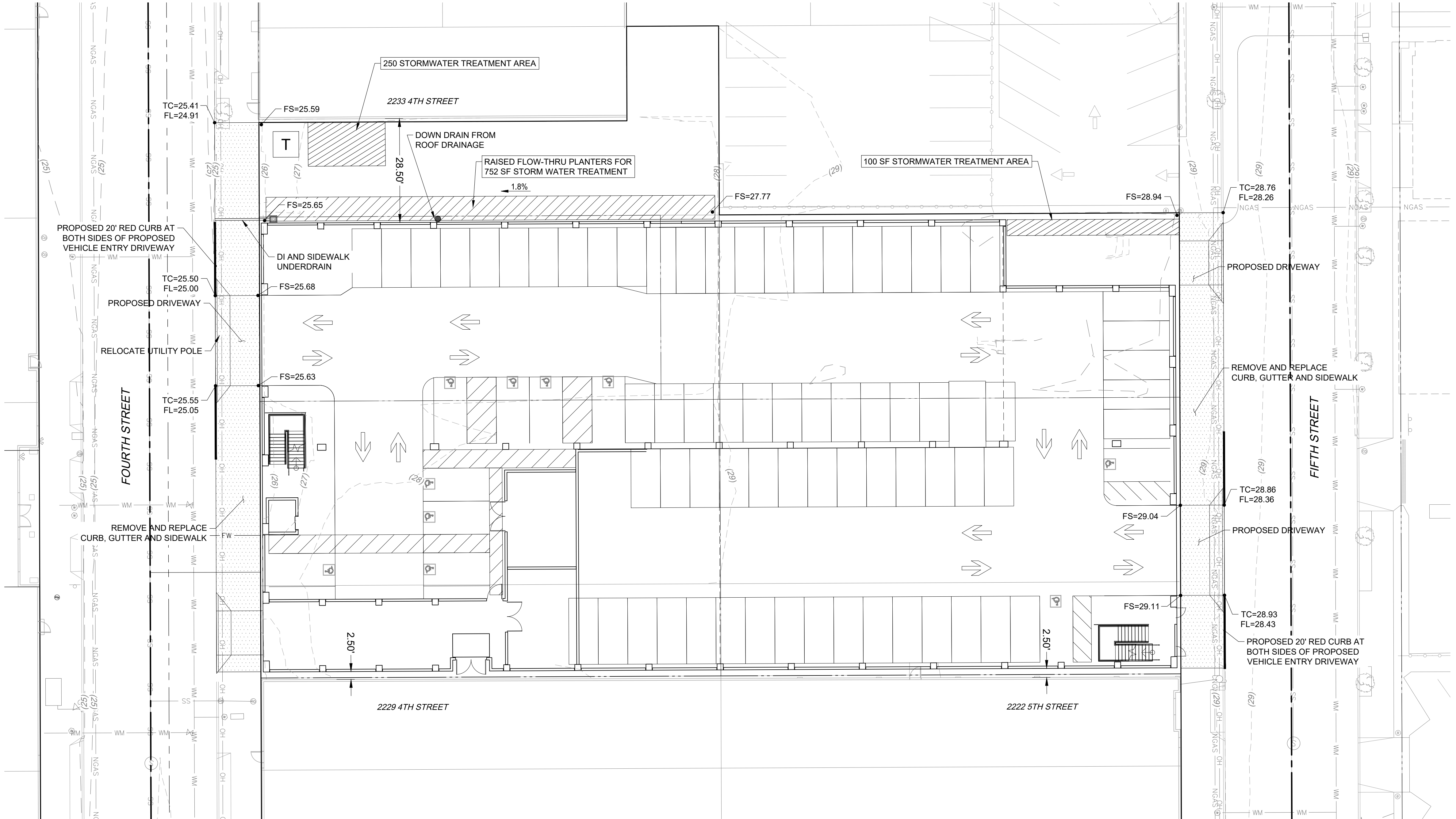
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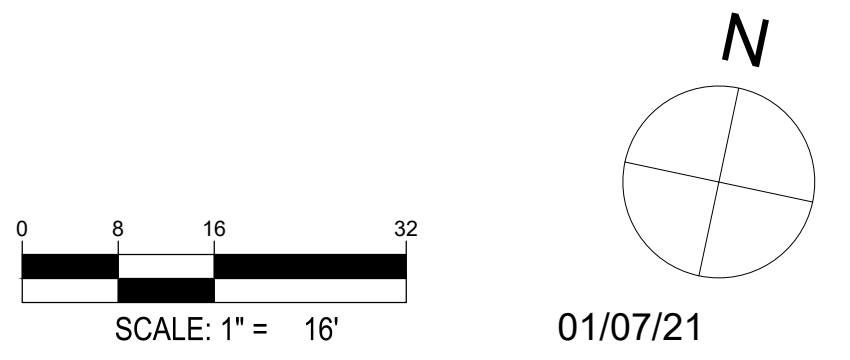
CONCEPTUAL GRADING PLAN C-3



LEGEND:

	PROPERTY LINE
	PROPOSED 5' CONTOUR
	PROPOSED 1' CONTOUR
	EXISTING 5' CONTOUR
	EXISTING 1' CONTOUR
	PROPOSED STORM LINE
	EXISTING STORM LINE
	PROPOSED STORM INLET
	EXISTING STORM INLET
	PROPOSED SWALE
	PROPOSED CURB
	EXISTING CURB
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING NATURAL GAS LINE
	EXISTING SANITARY LINE
	EXISTING WATER LINE
	PROPOSED SPOT ELEVATION (AT FLOWLINE UNLESS OTHERWISE INDICATED)
	PROPOSED SLOPE AND DIRECTION
	FLOW DIRECTION
	HIGH POINT
	LOW POINT
	MATCH EXISTING
	GRADE BREAK
	TOP OF WALL
	FINISH GRADE AT WALL

3 CONCEPTUAL GRADING PLAN
SCALE: 1/16" = 1'-0"



01/07/21

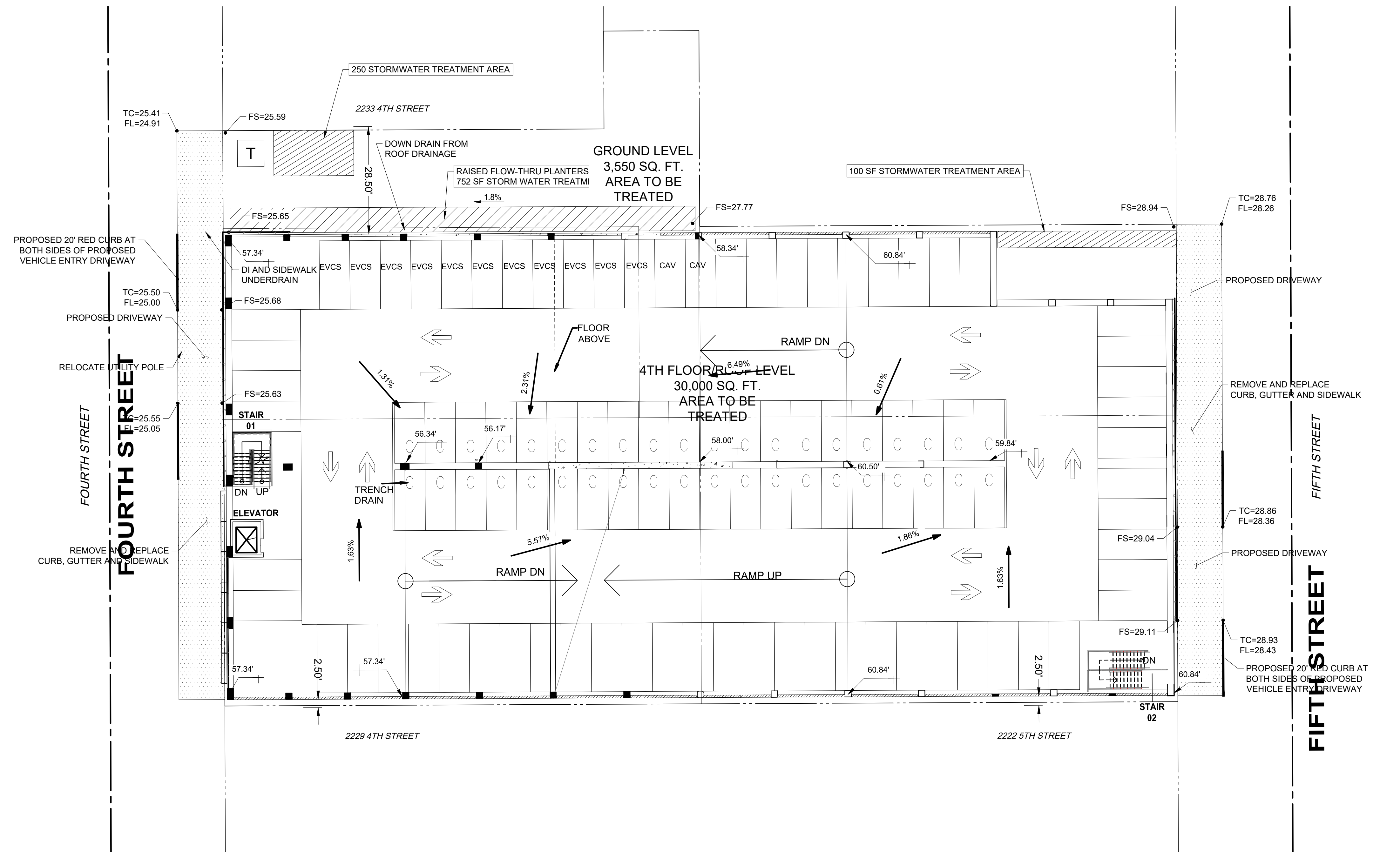
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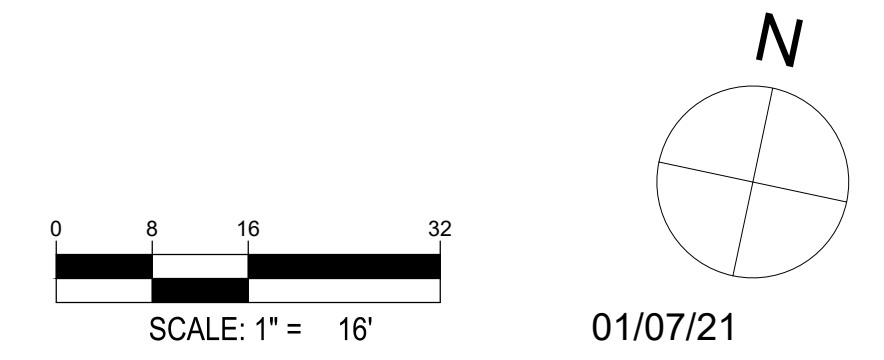
CONCEPTUAL STORM WATER MANAGEMENT PLAN | C-4



LEGEND:

—	PROPERTY LINE
—30—	PROPOSED 5' CONTOUR
—29—	PROPOSED 1' CONTOUR
-(30)-	EXISTING 5' CONTOUR
-(29)-	EXISTING 1' CONTOUR
—SD—	PROPOSED STORM LINE
—SD—	EXISTING STORM LINE
■	PROPOSED STORM INLET
■	EXISTING STORM INLET
→	PROPOSED SWALE
—	PROPOSED CURB
—	EXISTING CURB
—OH—	EXISTING OVERHEAD ELECTRIC LINE
—NGAS—	EXISTING NATURAL GAS LINE
—SS—	EXISTING SANITARY LINE
—WM—	EXISTING WATER LINE
•XX.XX	PROPOSED SPOT ELEVATION (AT FLOWLINE UNLESS OTHERWISE INDICATED)
X.XX%	PROPOSED SLOPE AND DIRECTION
→	FLOW DIRECTION
HP	HIGH POINT
LP	LOW POINT
ME	MATCH EXISTING GRADE BREAK
TOW	TOP OF WALL
FGW	FINISH GRADE AT WALL

4 CONCEPTUAL STORM WATER MANAGEMENT PLAN
SCALE: 1/16" = 1'-0"



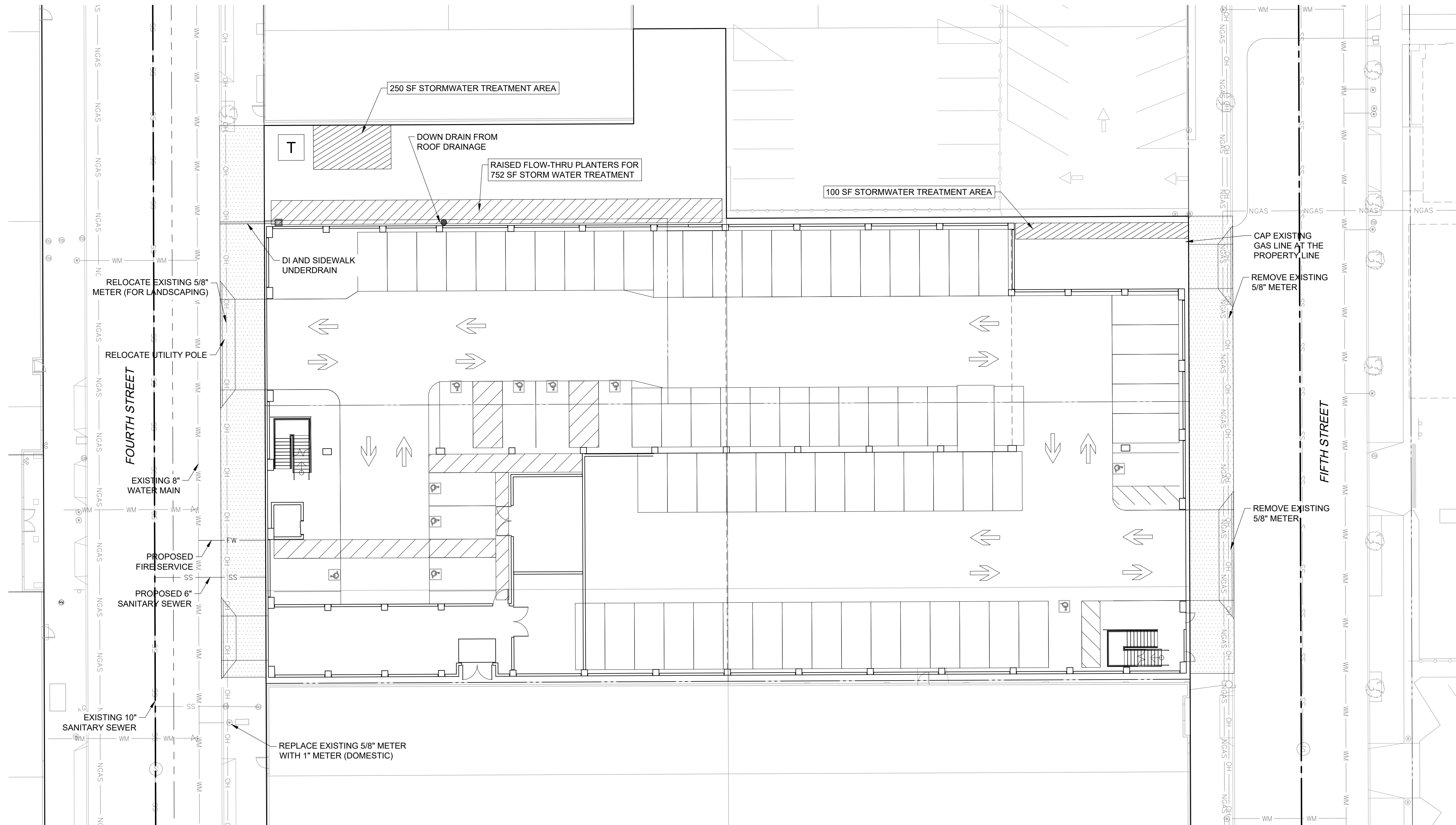
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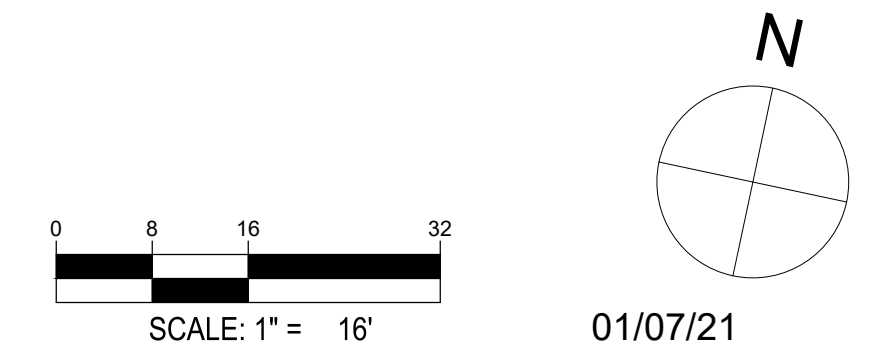
CONCEPTUAL UTILITY PLAN | C-5



LEGEND:

—	PROPERTY LINE
— 30 —	PROPOSED 5' CONTOUR
— 29 —	PROPOSED 1' CONTOUR
- (30) -	EXISTING 5' CONTOUR
- (29) -	EXISTING 1' CONTOUR
— SD —	PROPOSED STORM LINE
— SD —	EXISTING STORM LINE
■	PROPOSED STORM INLET
■	EXISTING STORM INLET
→	PROPOSED SWALE
—	PROPOSED CURB
—	EXISTING CURB
— OH —	EXISTING OVERHEAD ELECTRIC LINE
— NGAS —	EXISTING NATURAL GAS LINE
— SS —	EXISTING SANITARY LINE
— WM —	EXISTING WATER LINE
• XX.XX	PROPOSED SPOT ELEVATION (AT FLOWLINE UNLESS OTHERWISE INDICATED)
X.XX%	PROPOSED SLOPE AND DIRECTION
→	FLOW DIRECTION
HP	HIGH POINT
LP	LOW POINT
ME	MATCH EXISTING
GB	GRADE BREAK
TOW	TOP OF WALL
FGW	FINISH GRADE AT WALL

5 CONCEPTUAL UTILITY PLAN
SCALE: 1/16" = 1'-0"



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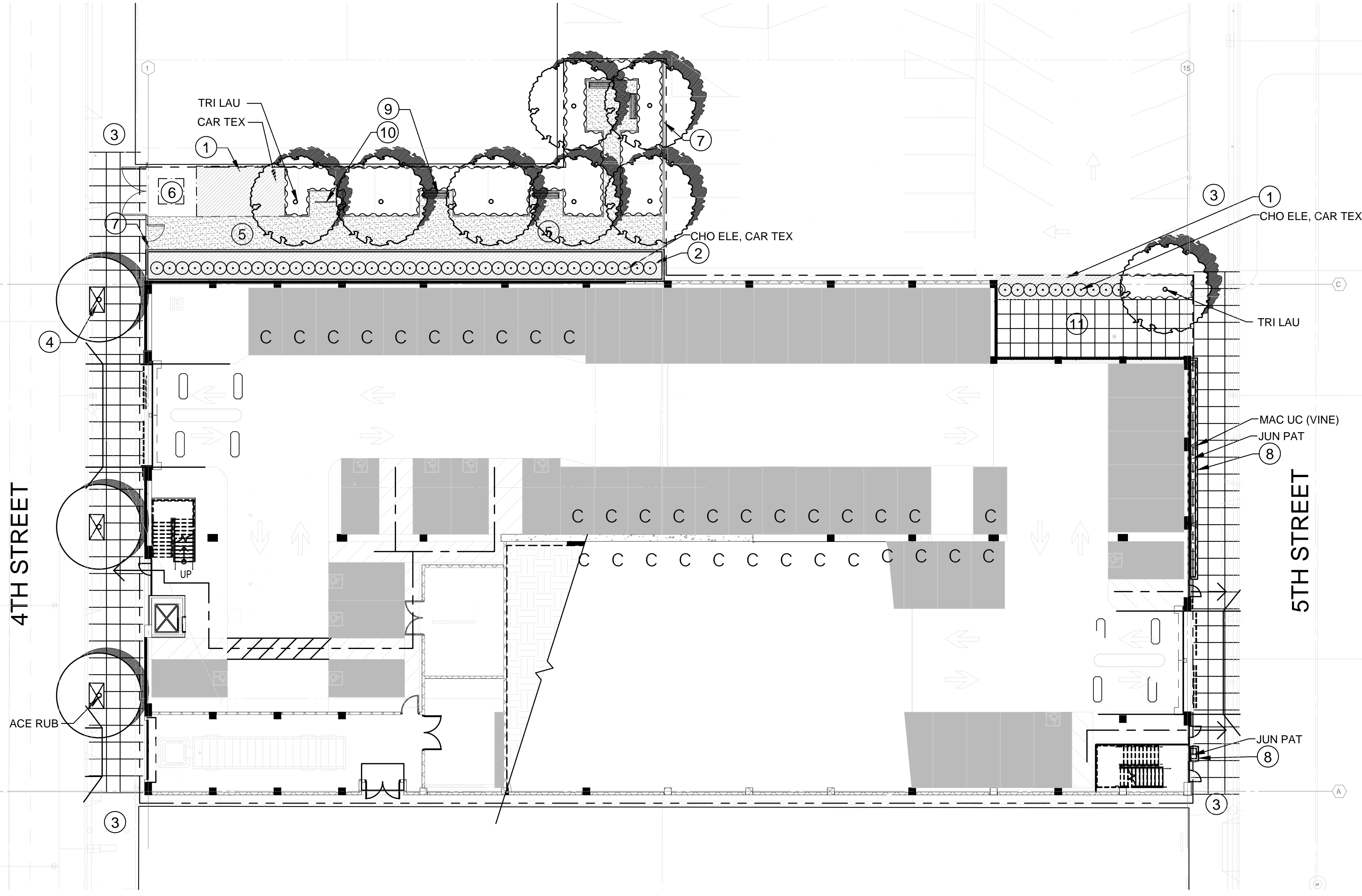
LANDSCAPE PLAN L1

KEY NOTES

- ① STORMWATER TREATMENT AREA
- ② STORMWATER TREATMENT AREA--FLOW-THROUGH PLANTER
- ③ CITY SIDEWALK
- ④ TREE WELL: 3.5 FT X 6 FT
- ⑤ DG PAVING
- ⑥ TRANSFORMER PAD
- ⑦ FENCE/GATE
- ⑧ RAISED CONCRETE PLANTER AT FACE OF BUILDING
- ⑨ BENCH
- ⑩ BIKE RACK
- ⑪ MOTORCYCLE PARKING

PLANT LIST

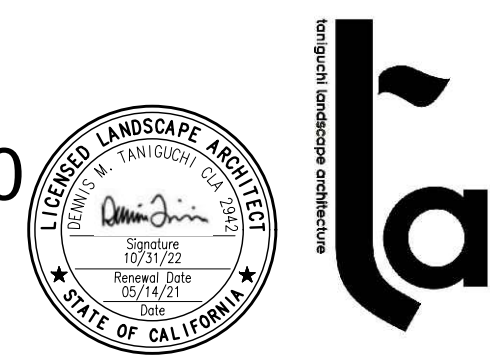
ABBREV.	BOTANICAL NAME	COMMON NAME	SIZE	MSC. NOTES & REQUIREMENTS
TREES				
ACE RUB	<i>Acer rubrum 'October Glory'</i>	Red Maple	24" box	S.L.No. Whort. Br./N. Dip. Br./Match
TRI LAU	<i>Tristania laurina 'Elsgart'</i>	Tristania	24" box	S.L.No. Whort. Br./N. Dip. Br./Match
SHRUBS/PERENNIALS				
CAR TEX	<i>Carex texensis</i>	Catlin Sedge	1 G.C.	Plant at 15" o.c.
CHO ELE	<i>Chondropetalum elephantinum</i>	Large Cape Rush	1 G.C.	
JUN PAT	<i>Juncus patens</i>	California Grey Rush	1 G.C.	
NAN GS	<i>Nandina domestica 'Gulf Stream'</i>	Dwarf Heavenly Bamboo	5 G.C.	F & B
VINES				
MAC UC	<i>Macfadyena unguis-cati</i>	Cat's Claw Creeper	5 G.C.	
PLANT LIST ABBREVIATIONS:				
Note: This list together with the plant list prepared by Taniguchi Landscape Architecture must accompany the contractor's nursery order(s).				
SL	Single main, straight, dominant, leader			
Hl. Br.	High branched—lowest limbs held above rootball 5' min. for 15 gallon can 6' min. for 24" box trees			
F & B	Full dense, bushy, vigorous plants, with young growth closely spaced on branches, no old/woody plants.			
No. Whort. Br.	No closely spaced whorled branches. Select even symmetrical branch distribution			
Match	Matched size, form, caliper, branching and cultivar. Select from one lot, one grower, for guaranteed consistency through life of plants.			
	In general plants within a group or area are to be matched, unless noted otherwise.			
G.C.	On center			
N. Dip. Br.	No long heavy drooping branches			



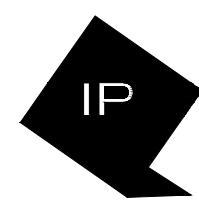
① LANDSCAPE PLAN
SCALE: 1/16" = 1'-0"

TheLAB Parking Structure

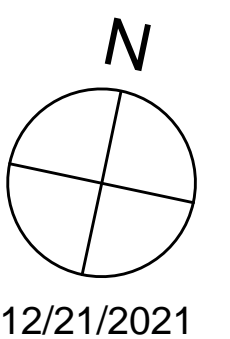
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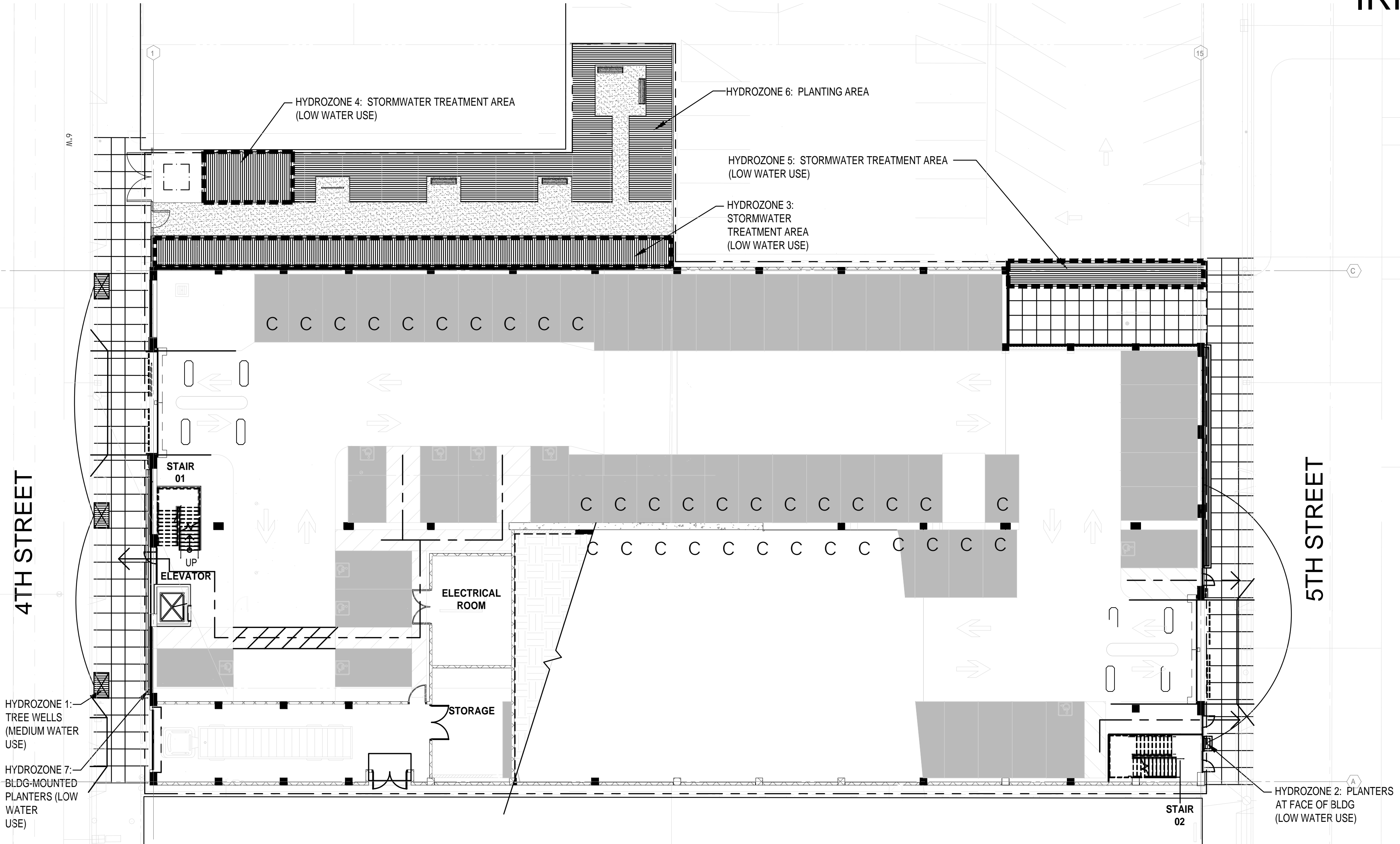
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IRRIGATION HYDROZONE PLAN L2



HYDROZONE LEGEND

- LOW WATER USE (1306 SF OR 45% OF PLANTING AREA) (SUBSURFACE DRIP AND/OR DRIP EMITTERS)
- MEDIUM WATER USE (1615 SF OR 55% OF PLANTING AREA) (SUBSURFACE DRIP AND/OR DRIP EMITTERS)
- HIGH WATER USE (0 SF OR 0% OF PLANTING AREA)

MWEL IRRIGATION CALCULATIONS

Water Efficient Landscape Worksheet: LAB Parking Structure (12/21/2021)

Reference Evapotranspiration (ET _o)	41.8 (Oakland)						Estimated Total Water Use (ETWU)
	ETWU requirement	ETWU requirement	ETWU requirement	ETWU requirement	MAWA requirement	ETWU requirement	
Hydrozone#/Planting Description	Plant Factor (PF)	Irrigation Method	Irrigation Efficiency (IE)	ETAF (PF/IE)	Landscape Area (LA) (sq. ft.)	ETAF x Area	
Regular Landscape Areas							
#1 Tree Wells	0.5	Drip	0.81	0.617	63	38.89	1,008
#2 Planters	0.2	Drip	0.81	0.247	81	20.00	518
#3 Stormwater Planter	0.2	Drip	0.81	0.247	793	195.80	5,074
#4 Stormwater	0.2	Drip	0.81	0.247	246	60.74	1,574
#5 Stormwater	0.4	Drip	0.81	0.494	277	136.79	3,545
#6 Trees	0.4	Drip	0.81	0.494	1,275	629.63	16,317
#7 Building Planters	0.2	Drip	0.81	0.247	176	43.46	1,126
Totals					2,911	1,125.31	29,163
Special Landscape Areas (SLA)							
						0	0
						0	0
						0	0
Totals					0	0	0
Estimated Total Water Use (ETWU)						29,163	
Maximum Allowed Water Allowance (MAWA)						41,493	

Plant Water Use Type	Plant Factor	Irrigation method	Irrigation Efficiency
very low	0-0.1	overhead spray	0.75
low	0.1-0.3	drip	0.81
medium	0.4-0.6		
high	0.7-1.0		

MAWA (annual gallons allowed) = (Eto) (0.62) [(ETAF x LA) + ((1-ETAF) x SLA)]

where 0.62 is a conversion factor that converts acre-inches per acre/year to gallons per sq. ft./year. LA is the total landscape area in sq. ft., SLA is the total special landscape area in sq. ft., and ETAF is .55 for residential areas and 0.45 for non residential areas.

1 IRRIGATION HYDROZONE PLAN

SCALE: 1/16" = 1'-0"

CONCEPTUAL IRRIGATION STATEMENT

- 1 Irrigation design shall be zoned for 1) turf and annuals and other moderate to higher water use plant materials; 2) groundcovers, and 3) native and water conserving plant materials.
- 2 Irrigation design shall also be zoned for micro climates including cool, shaded and protected areas, as well as hot, sunny and windy areas.
- 3 Part shade areas include moderate water use areas having morning and/or afternoon shade.
- 4 Cool and full shady areas include low water use areas for plants requiring little or no irrigation water and/or locations that will provide moist conditions.
- 5 Layout shall be designed for minimum runoff and overspray onto non-landscaped areas
- 6 Low volume sprinklers shall be used wherever possible with head to head coverage.
- 7 Drip emitter or bubbler irrigation shall be utilized at trees to promote deep watering wherever possible.
- 8 Drip irrigation shall be utilized at non-traffic or isolated planting areas to decrease the possibility of vandalism to the micro-tubing.
- 9 The irrigation controller shall have ample capacity in terms of programs and cycles that will match the complexity of the landscape plan for more efficient watering. For example, the controller shall have the ability to have multiple cycles to permit a number of short duration waterings that will allow water to soak into the soil rather than run off.
- 10 Individual bubblers or drip emitters shall be utilized to isolate water for plant materials and eliminate watering of "bare ground."

STANDARDS FOR IRRIGATION EQUIPMENT

- 1 Mainslines shall be 1120 pvc-schedule 40 for pipe size 1 1/2" and smaller, 1120 pvc-class 315 for pipe sizes 2" and 2 1/2", bell and ring pvc-class 160 for pipe sizes 3" and larger.
- 2 Lateral lines shall be 1120 pvc-class 200.
- 3 Depth of mainline: 24" of cover
Depth of lateral line: 18" of cover
Depth of pipe under paving: 24" of cover encased in a sleeve
- 4 Backflow preventer shall be a type approved by and installed per local codes.
- 5 Sprinklers shall have matched precipitation rates within each control valve circuit.
- 6 Precipitation rates for sprinklers shall match soil absorption rate.
- 7 Sprinklers shall have pressure compensating feature whenever possible to prevent fogging and misting and to prevent wind drift.
- 8 Sprinkler circuit shall have a check valve installed where necessary to minimize or prevent low head drainage.
- 9 Rain sensing override devices shall be installed with controller.

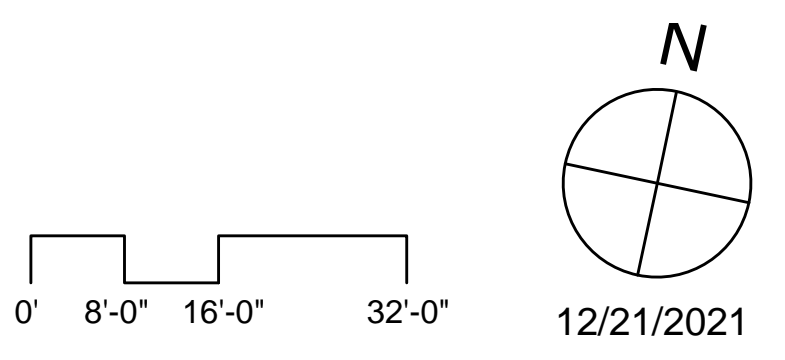
ETAF Calculations

Regular Landscape Areas	
Total ETAF x Area	1,125
Total Area	2,911
Average ETAF	0.39

Average ETAF for regular landscape areas must be 0.55 or below for residential areas, and 0.45 or below for non-residential areas.

All Landscape Areas

Total ETAF x Area	1,125
Total Area	2,911
Sitewide ETAF	0.39



TheLAB Parking Structure 2212/2216 5TH ST & 2213/2221 4TH ST. BERKELEY, CA 94510



Taniguchi Landscape Architecture
1013 South Claremont St., Ste 1
San Mateo, CA 94402
v 650.638.9885 | f 650.638.9886
CA 4242

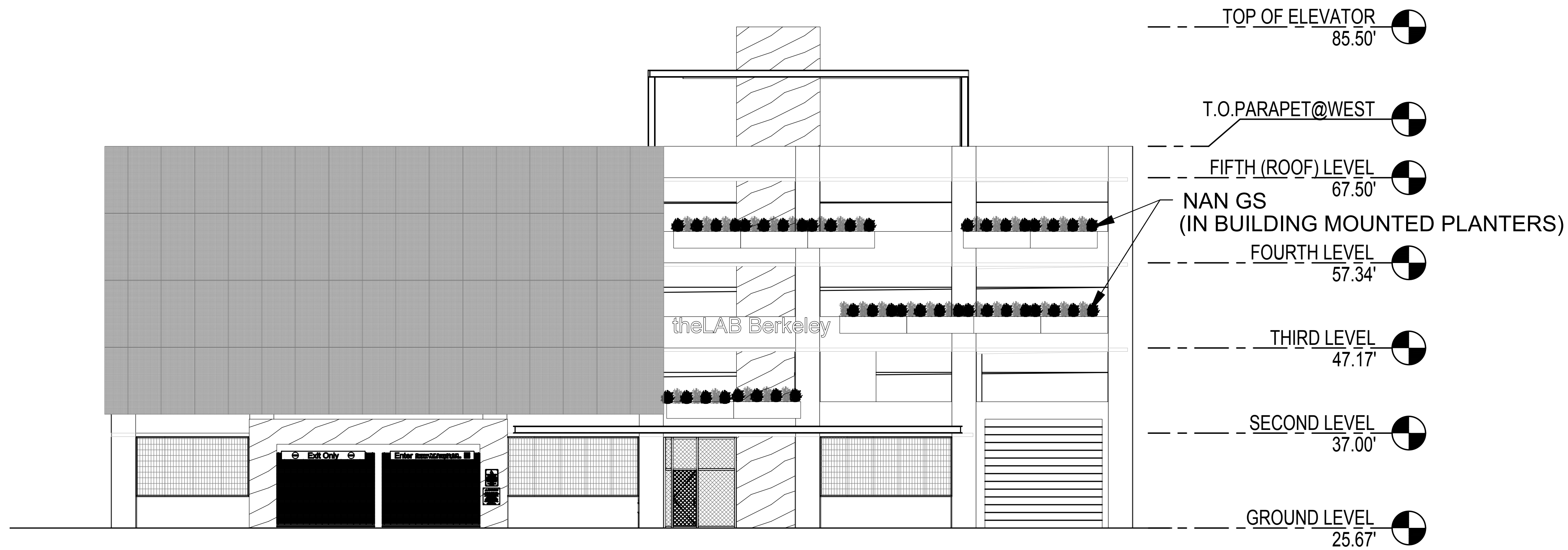


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560 14TH STREET, SUITE 300 OAKLAND CA 94612 T. 510.473.0300

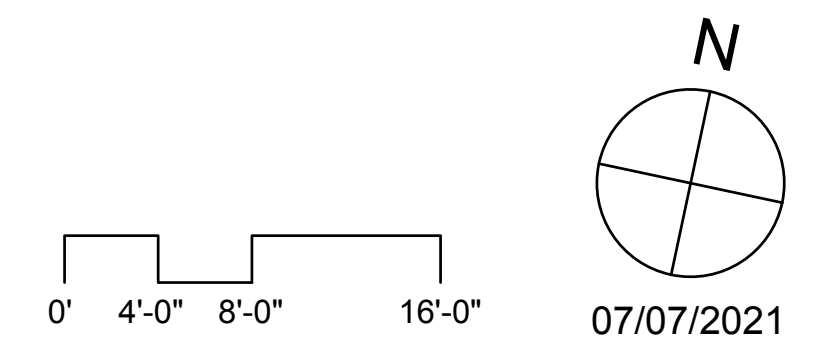
LANDSCAPE ELEVATION L3

NOTES

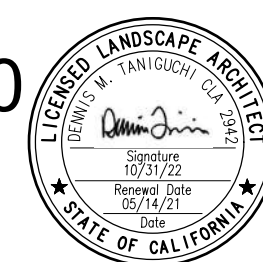
1. SEE PLANT LIST ON SHEET L1



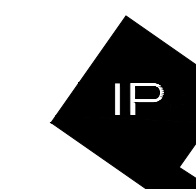
① LANDSCAPE ELEVATION: WEST ELEVATION (4TH STREET)
SCALE: 1/8" = 1'-0"



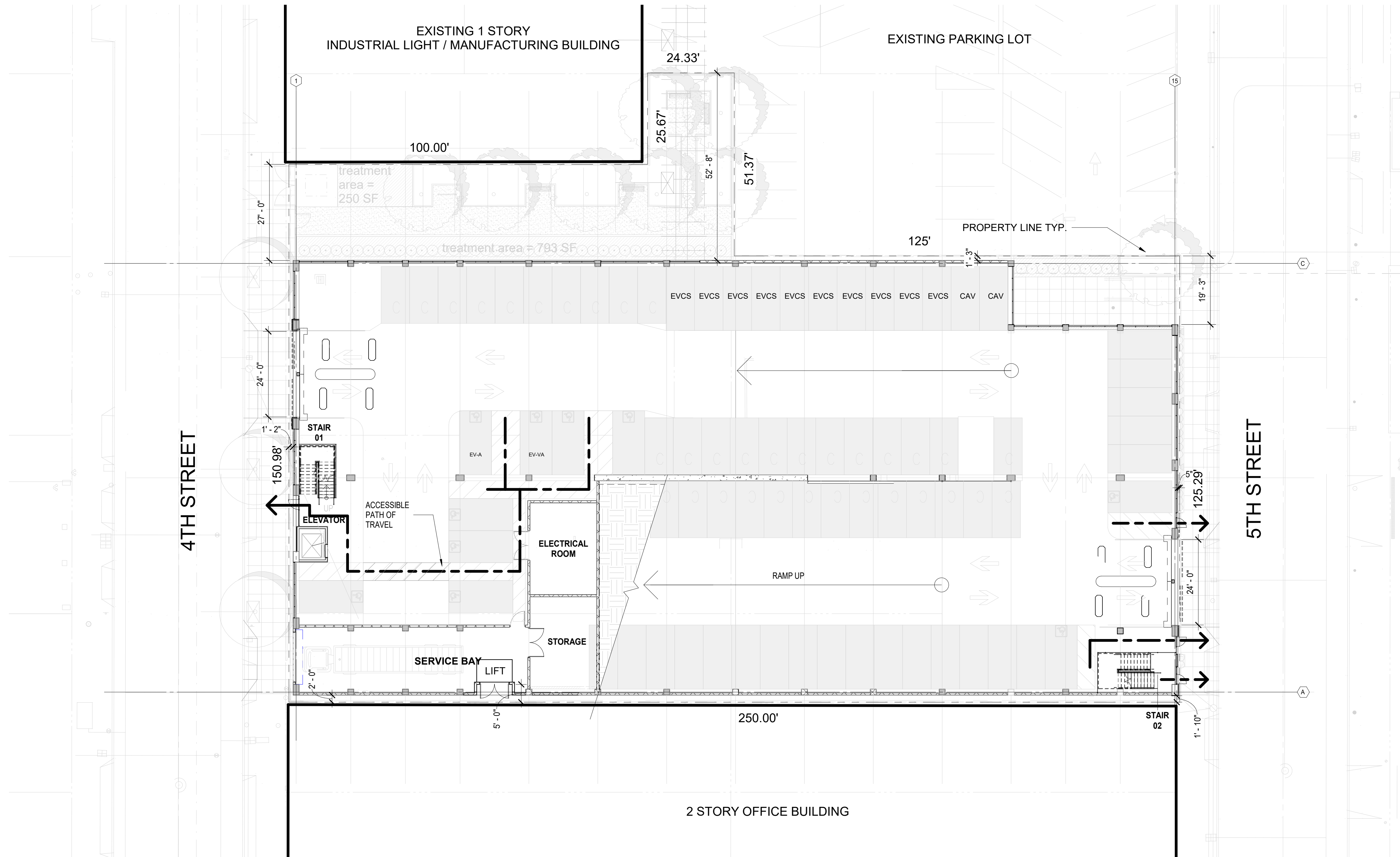
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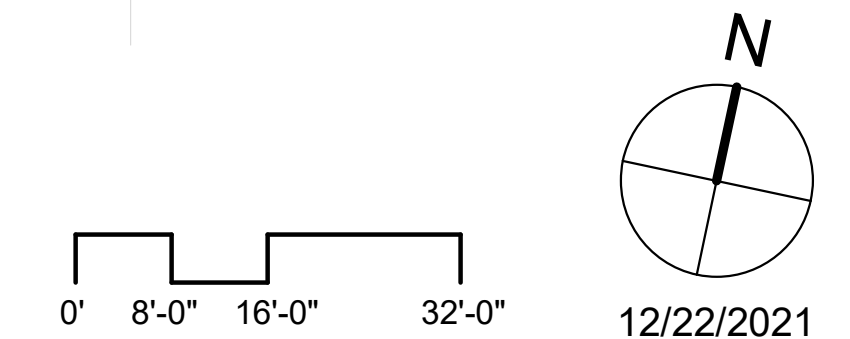
Taniguchi Landscape Architecture
1013 South Claremont St., Ste 1
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v 650.638.9985 | f 650.638.9986
CA 4582



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1 ARCHITECTURAL SITE PLAN
SCALE: 1/16" = 1'-0"

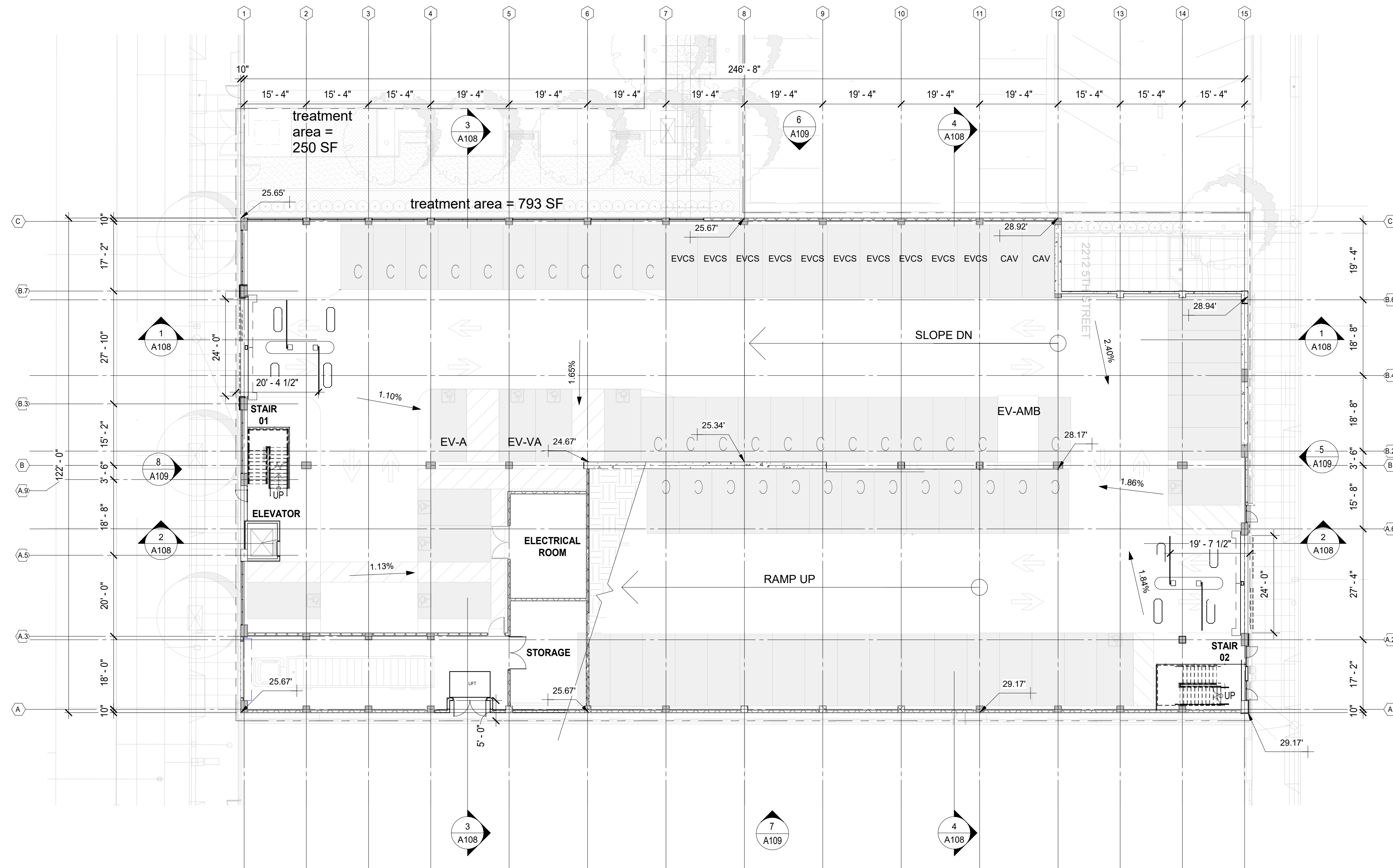


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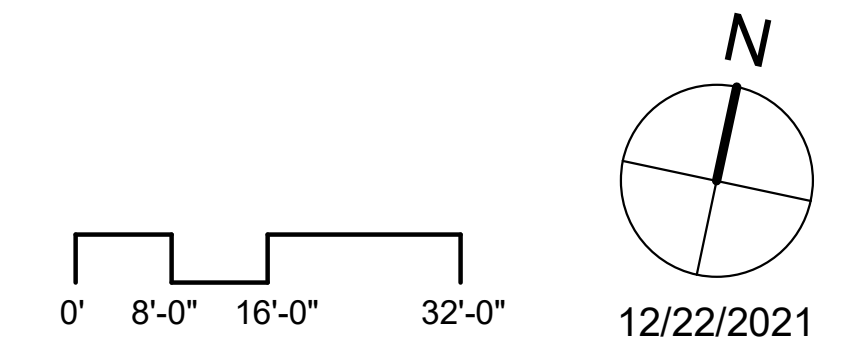


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GROUND LEVEL FLOOR PLAN | A103



1 GROUND LEVEL FLOOR PLAN
SCALE: 1/16" = 1'-0"

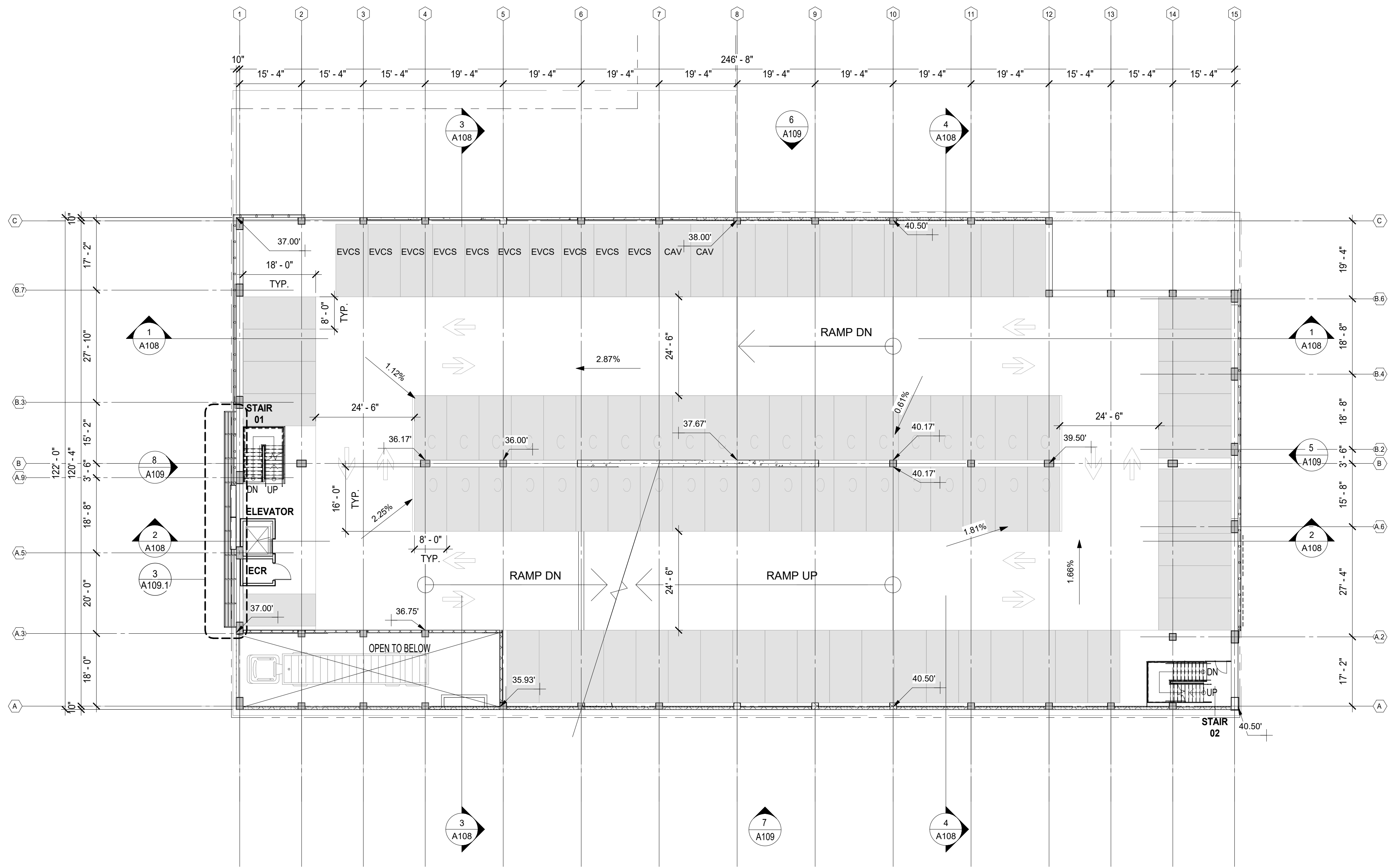


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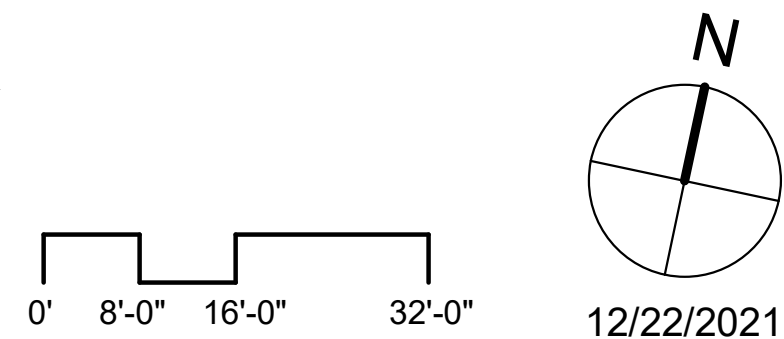


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SECOND LEVEL FLOOR PLAN | A104



1 SECOND LEVEL FLOOR PLAN
SCALE: 1/16" = 1'-0"

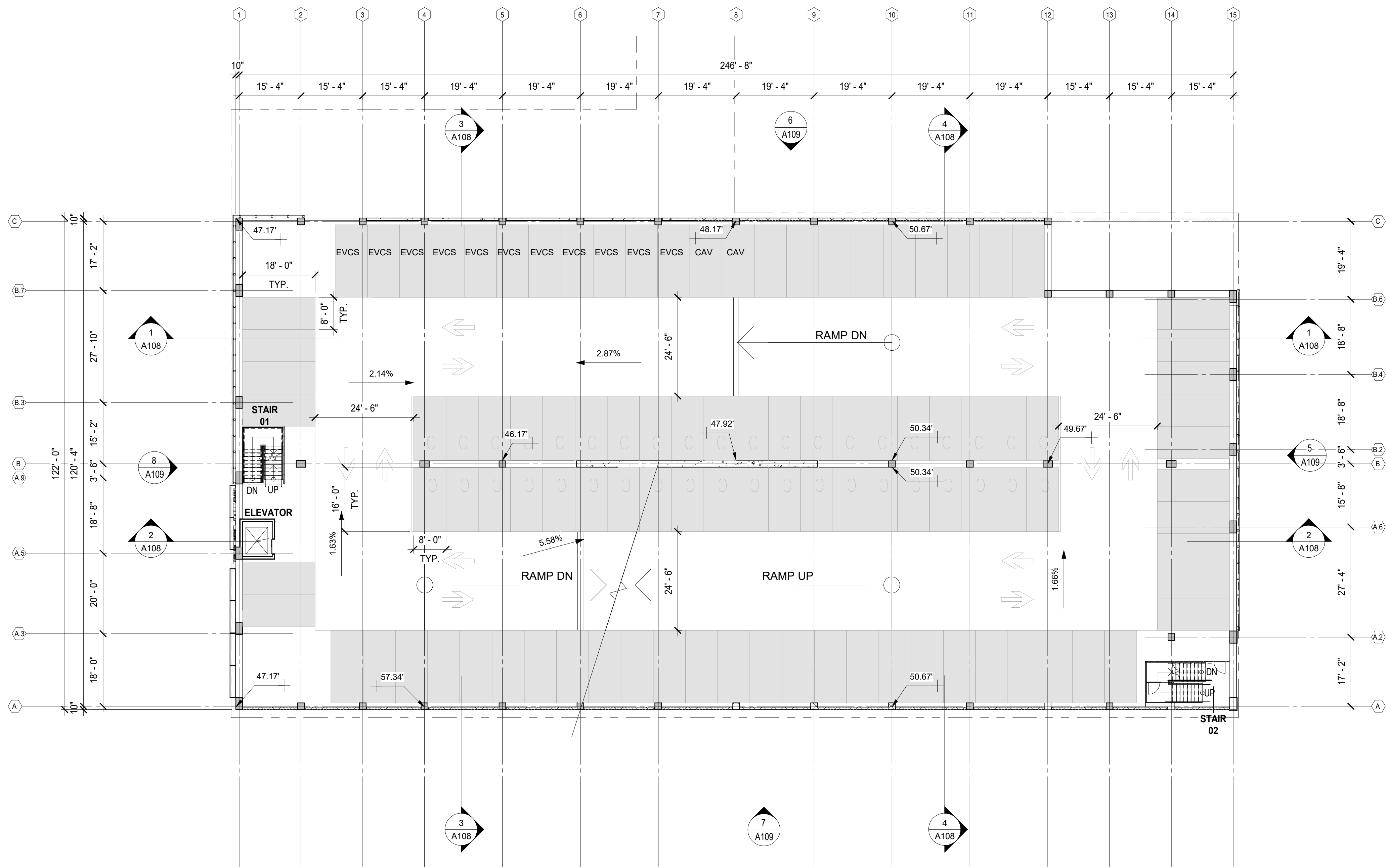


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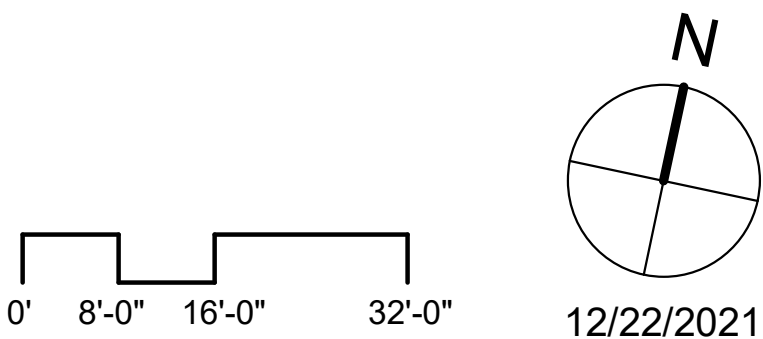


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THIRD LEVEL FLOOR PLAN | A105



1 THIRD LEVEL FLOOR PLAN
SCALE: 1/16" = 1'-0"

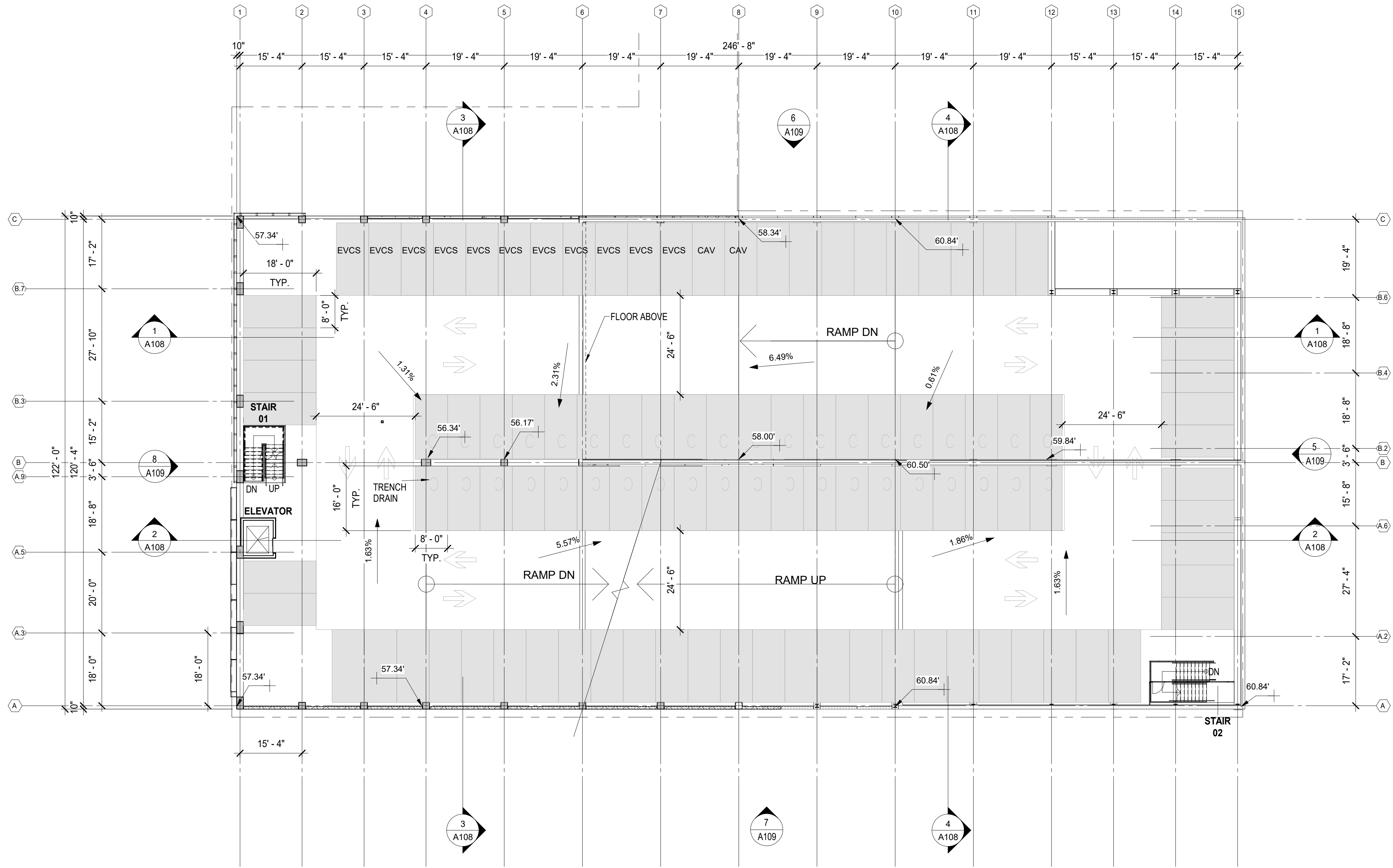


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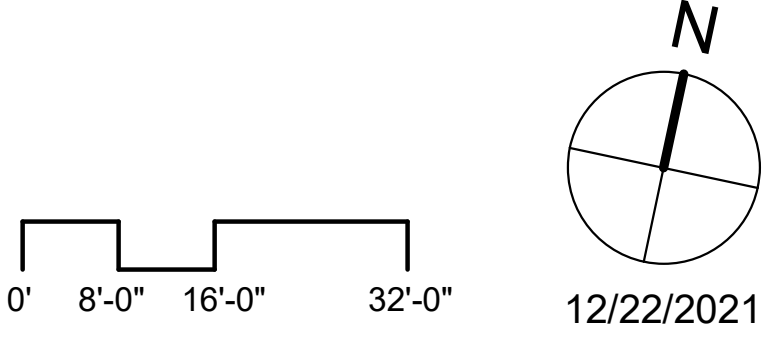


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FOURTH LEVEL FLOOR PLAN | A106



1 FOURTH LEVEL FLOOR PLAN
SCALE: 1/16" = 1'-0"

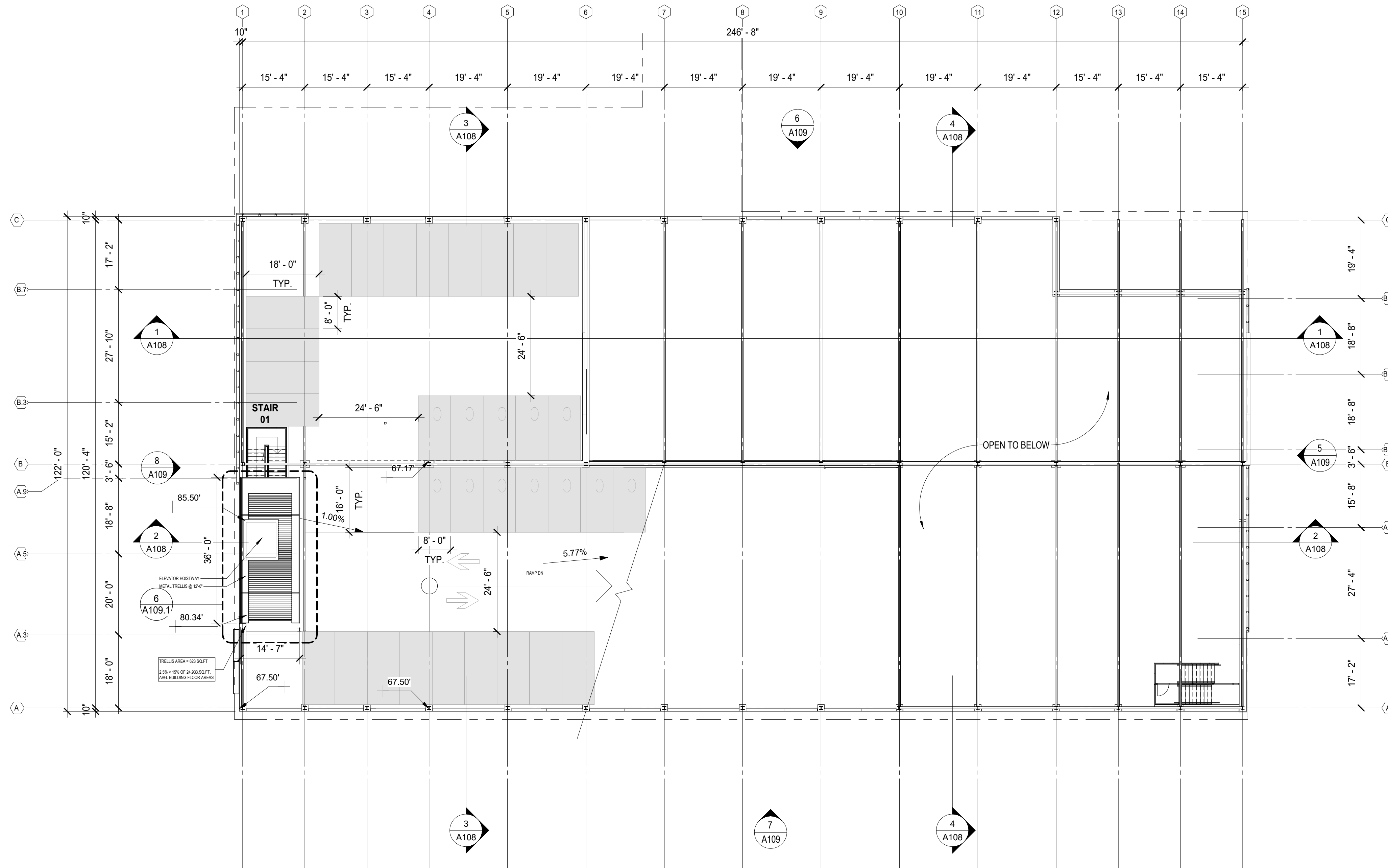


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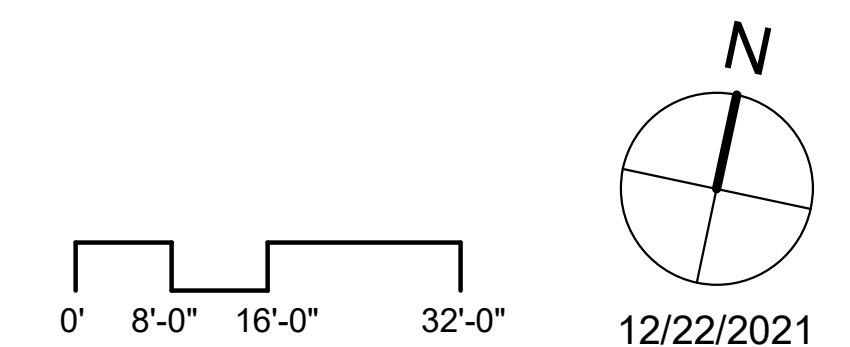


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FIFTH (ROOF) LEVEL FLOOR PLAN | A107



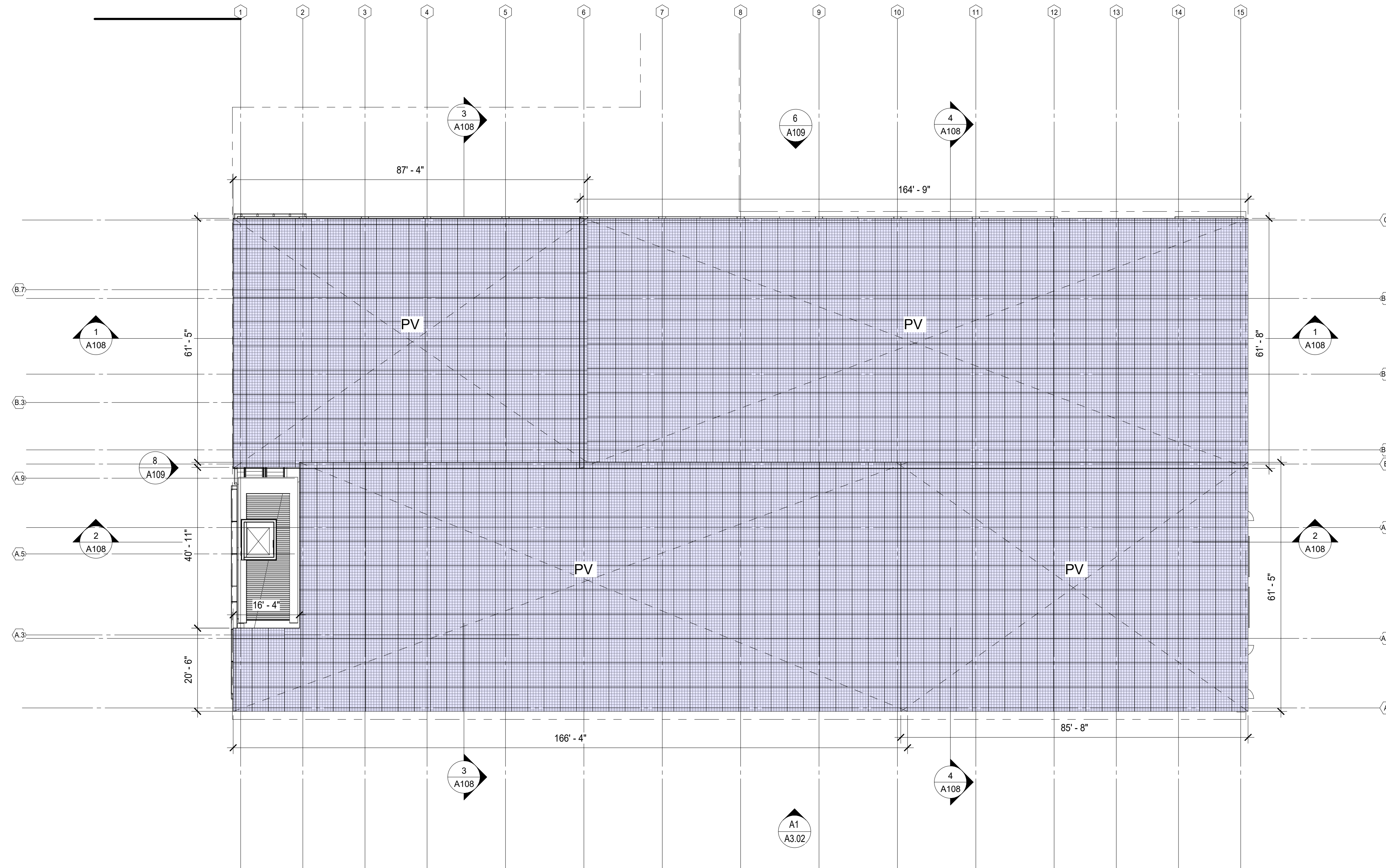
1 FIFTH (ROOF) LEVEL PLAN
SCALE: 1/16" = 1'-0"



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1 PV PLAN
SCALE: 1/16" = 1'-0"

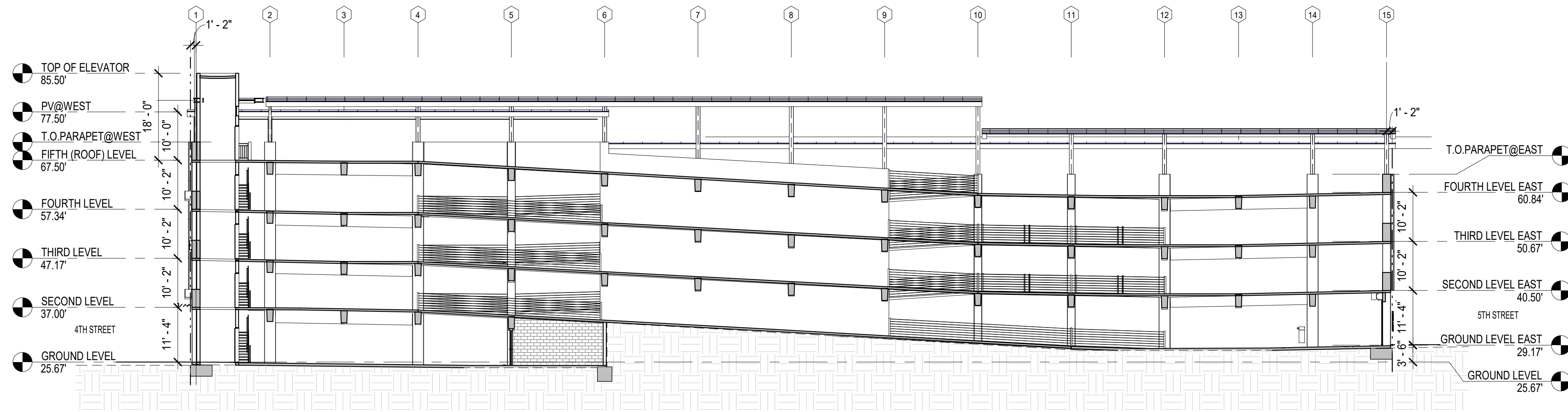
12/22/2021

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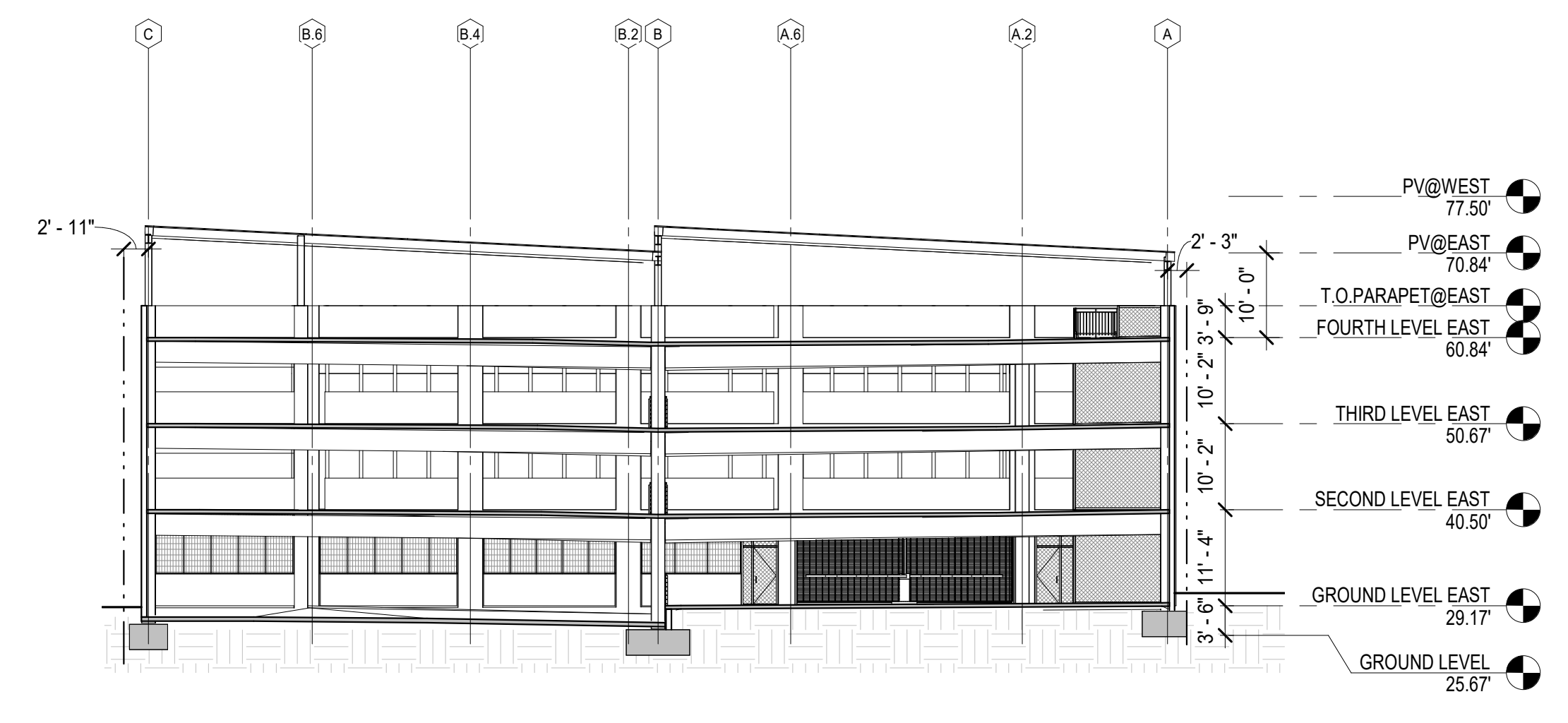


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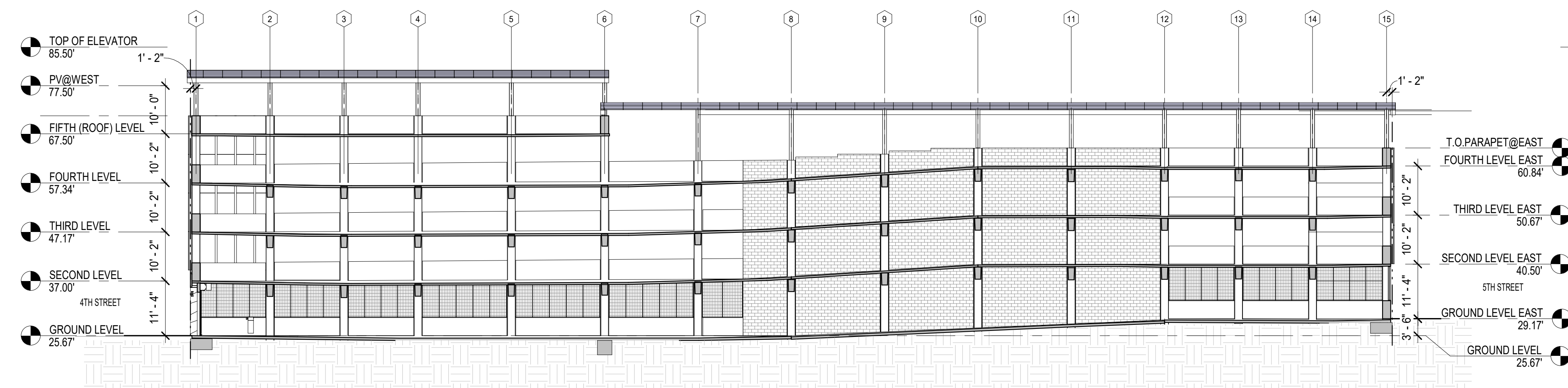
BUILDING SECTIONS | A108



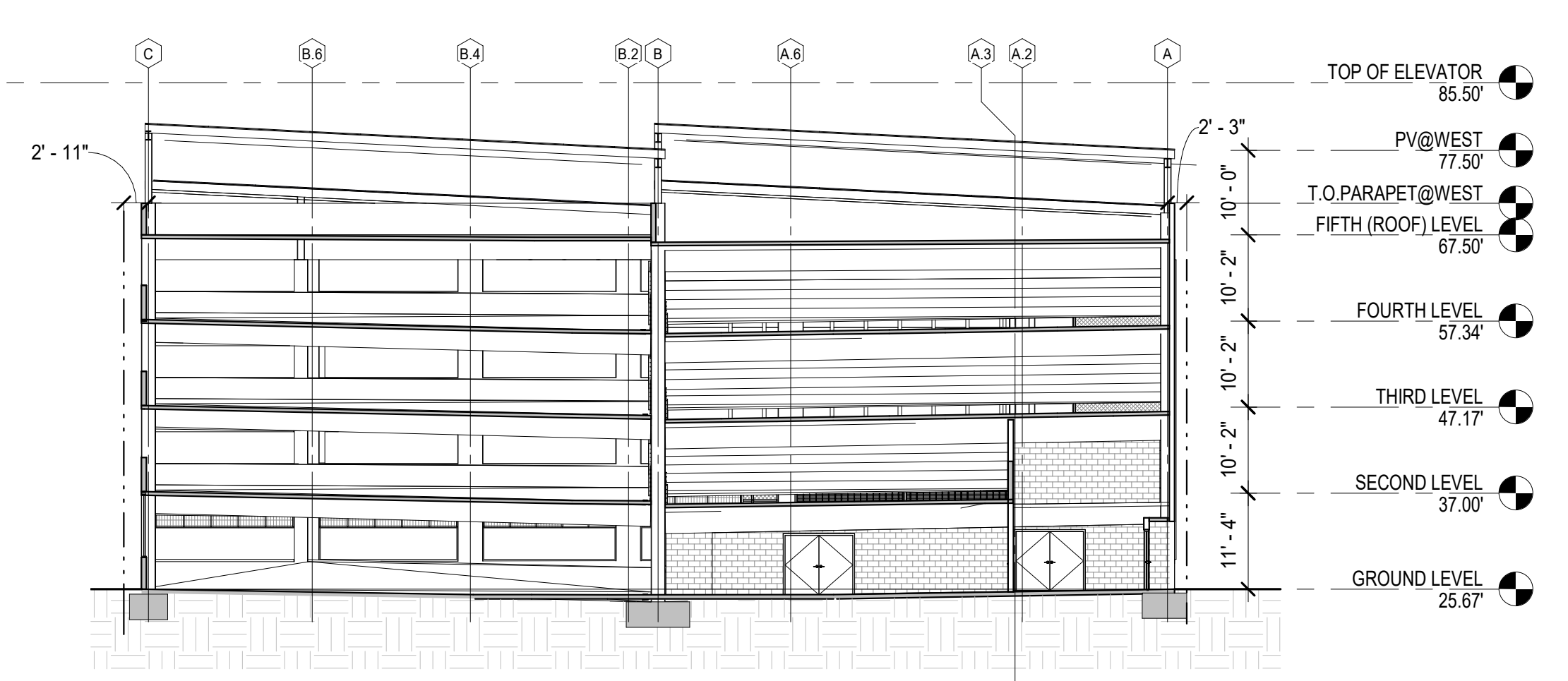
2 BUILDING SECTION 1
SCALE: 1/16" = 1'-0"



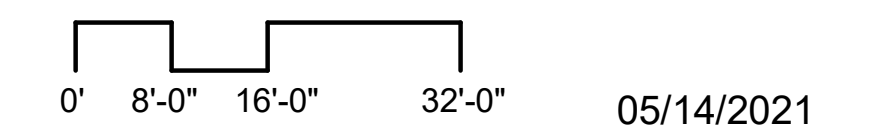
4 BUILDING SECTION 3
SCALE: 1/16" = 1'-0"



1 BUILDING SECTION 2
SCALE: 1/16" = 1'-0"



3 BUILDING SECTION 4
SCALE: 1/16" = 1'-0"

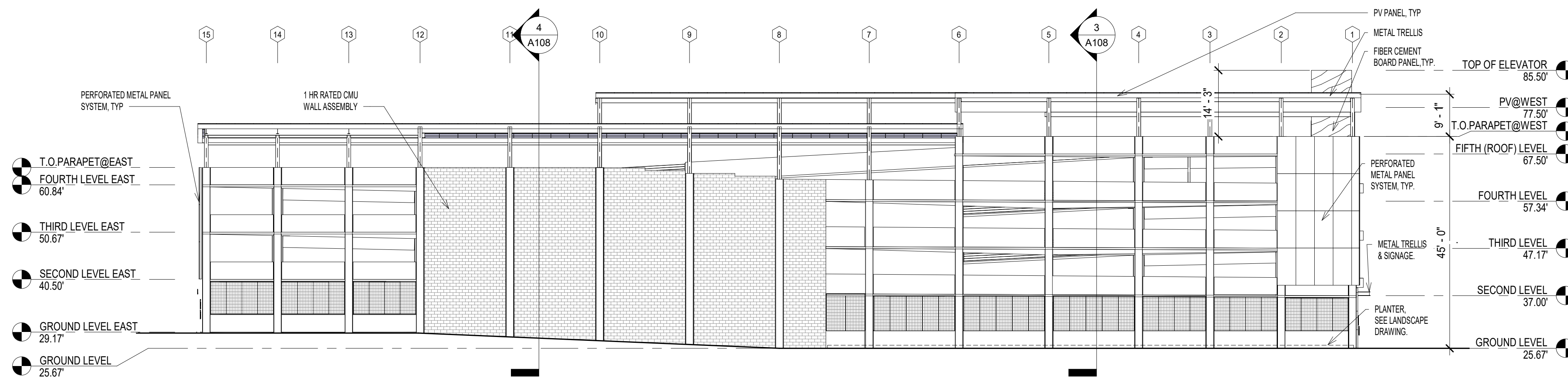


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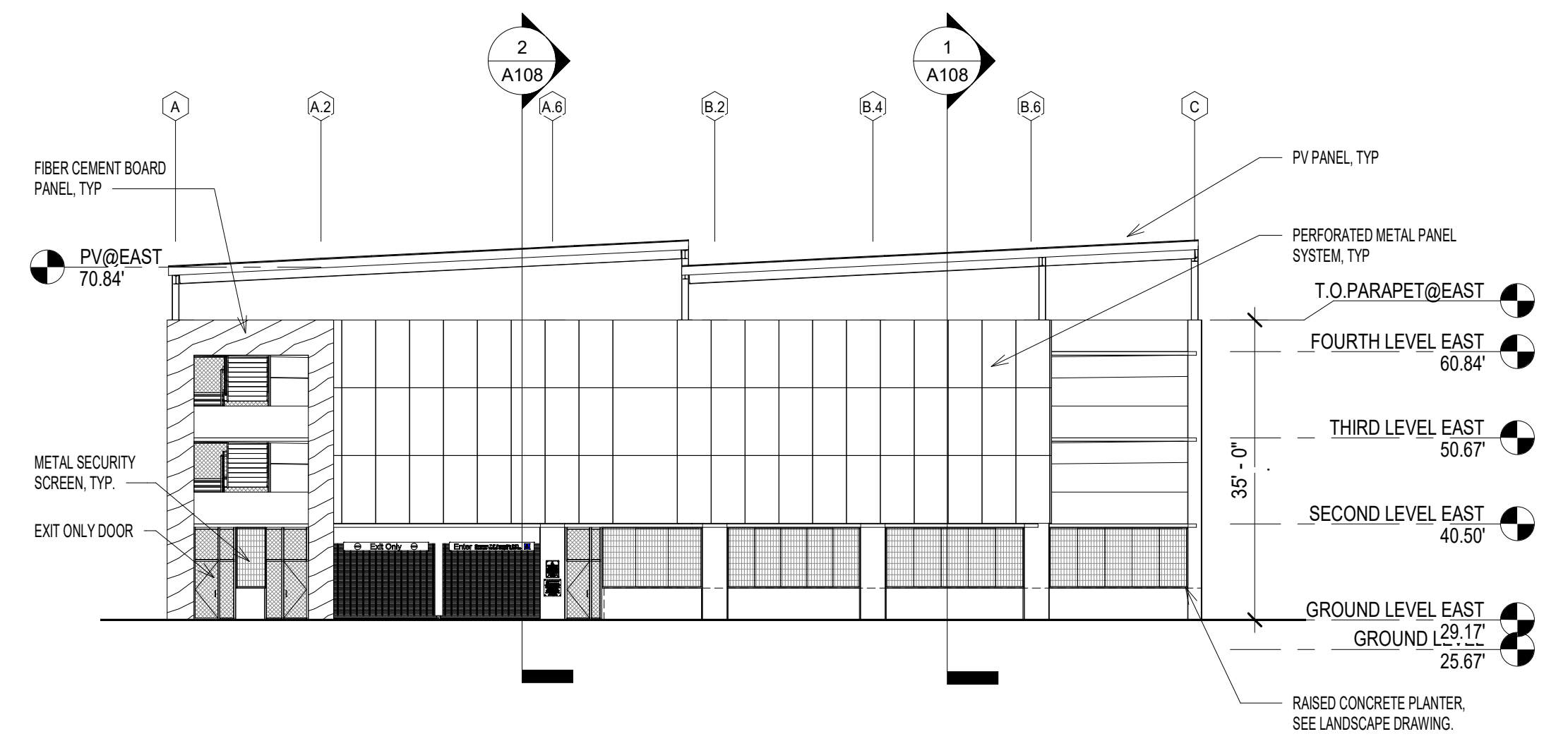


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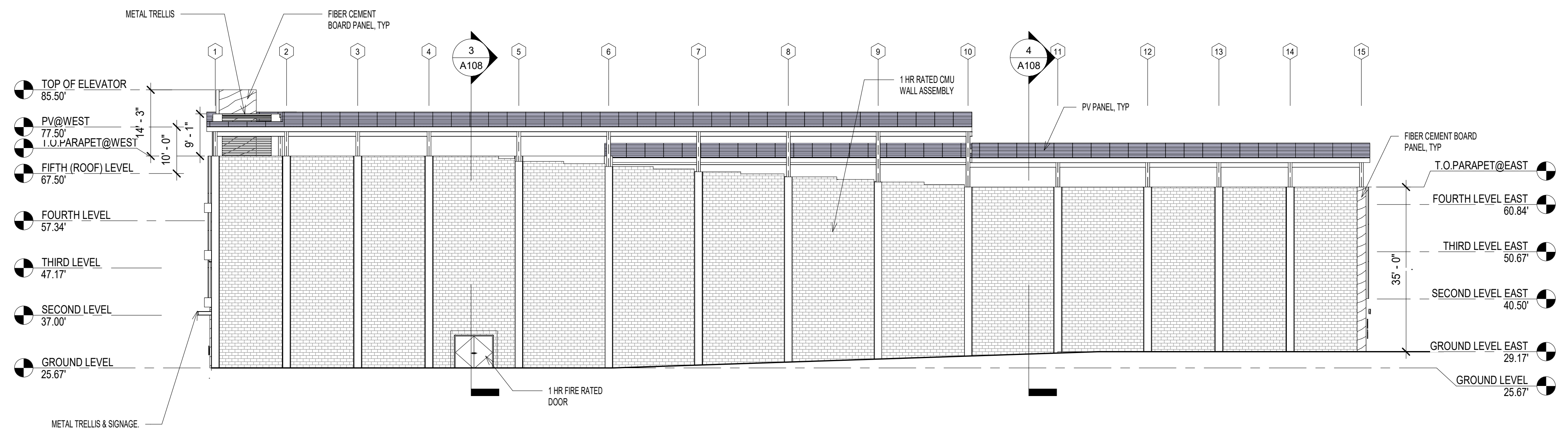
BUILDING ELEVATIONS | A109



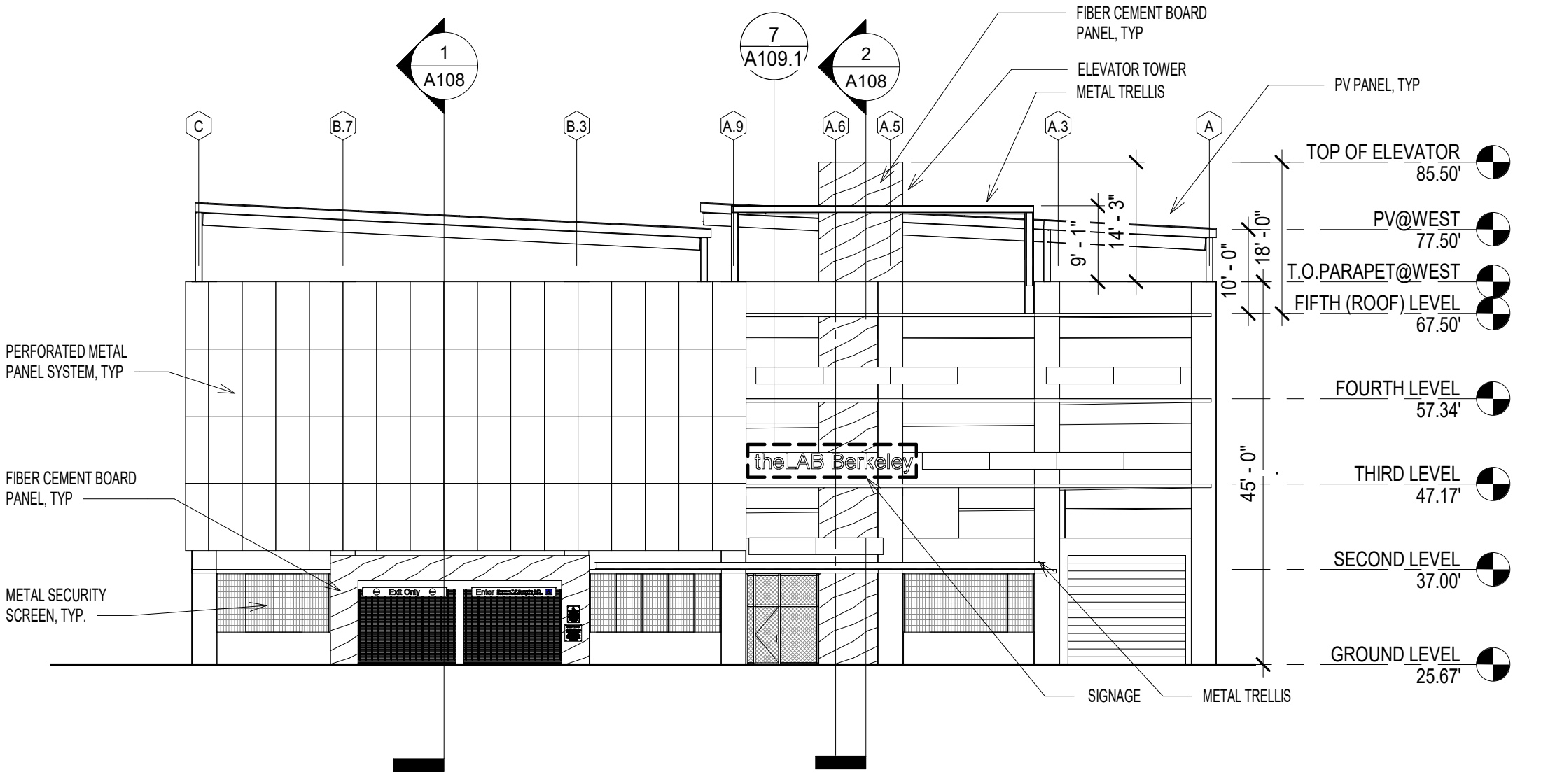
6 NORTH ELEVATION -
SCALE: 1/16" = 1'-0"



5 EAST ELEVATION -
SCALE: 1/16" = 1'-0"



7 SOUTH ELEVATION -
SCALE: 1/16" = 1'-0"



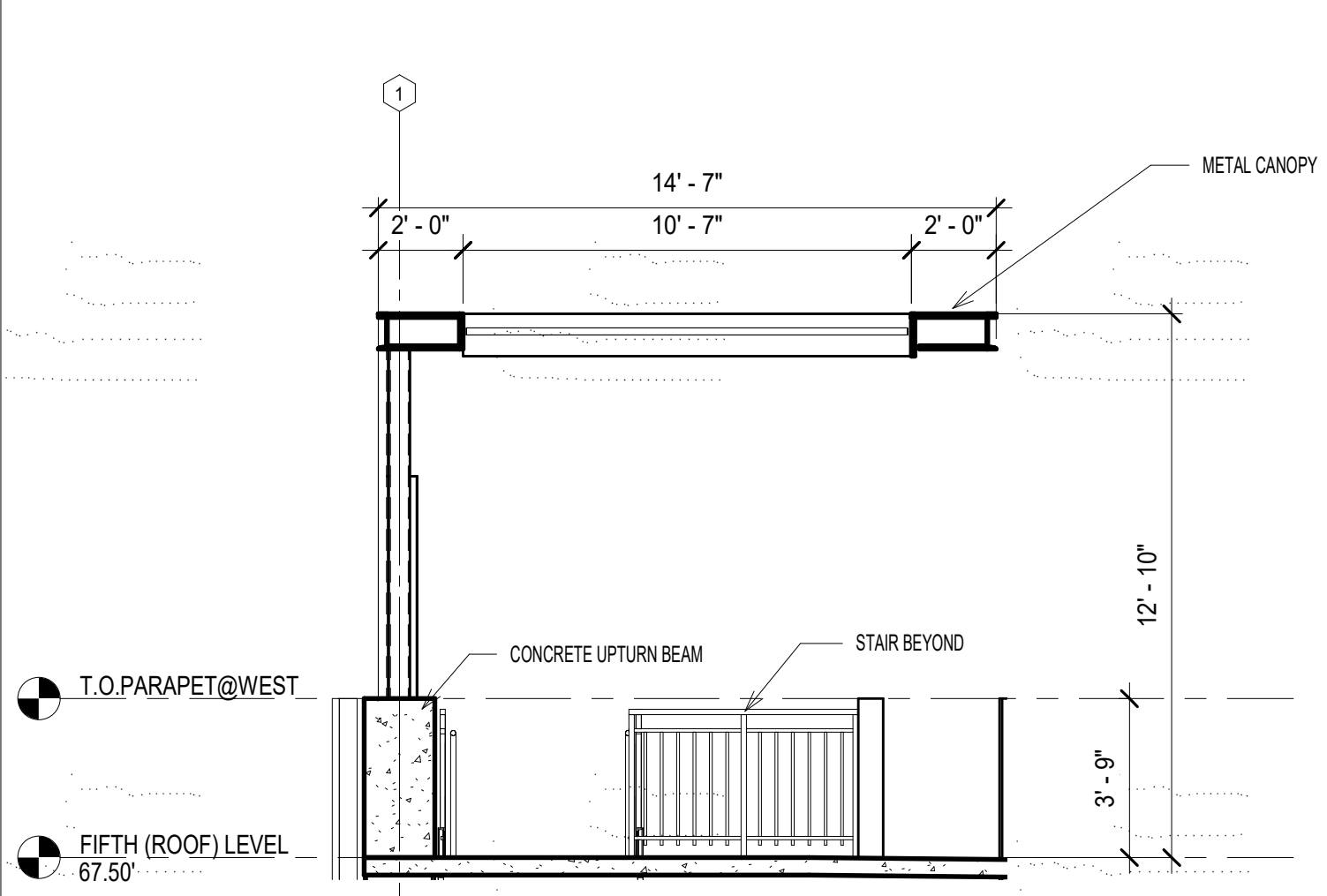
8 WEST ELEVATION -
SCALE: 1/16" = 1'-0"

0' 8'-0" 16'-0" 32'-0" 05/14/2021

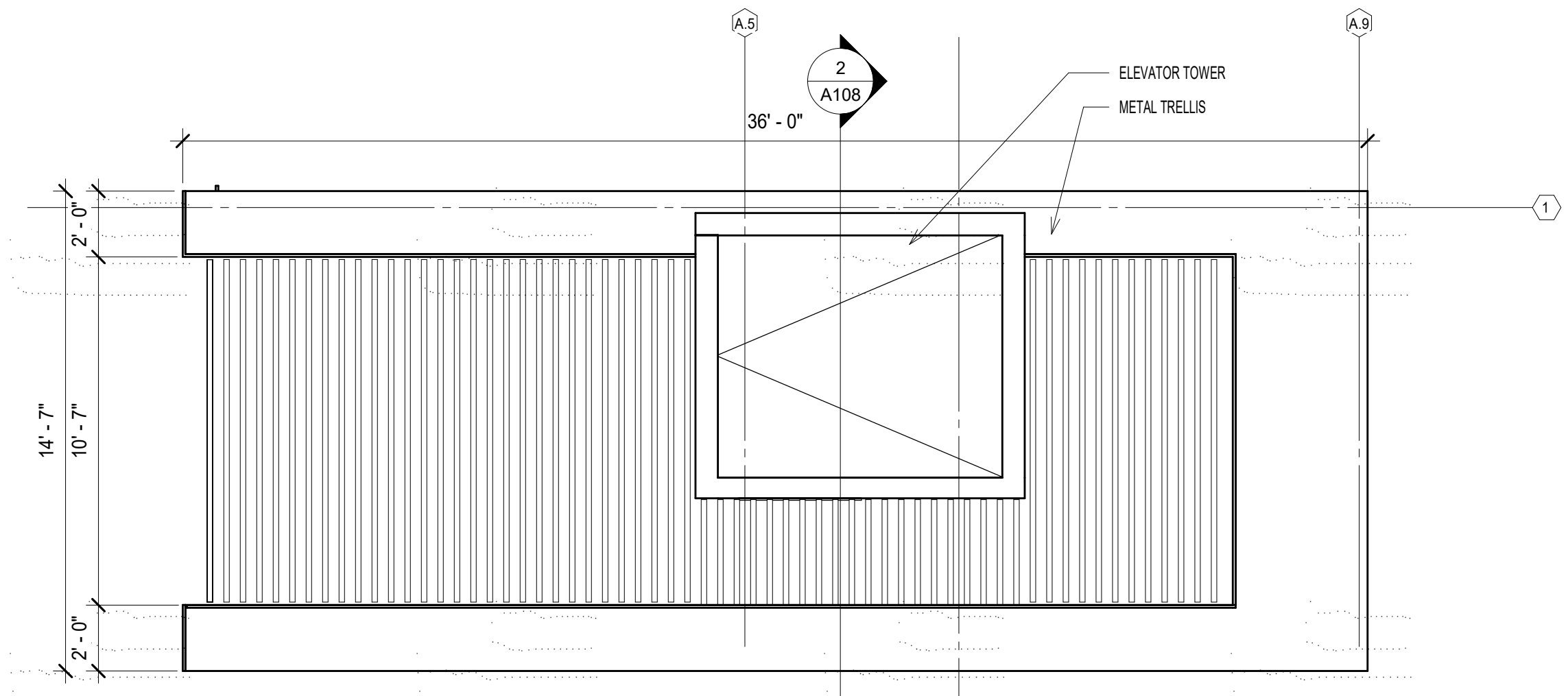
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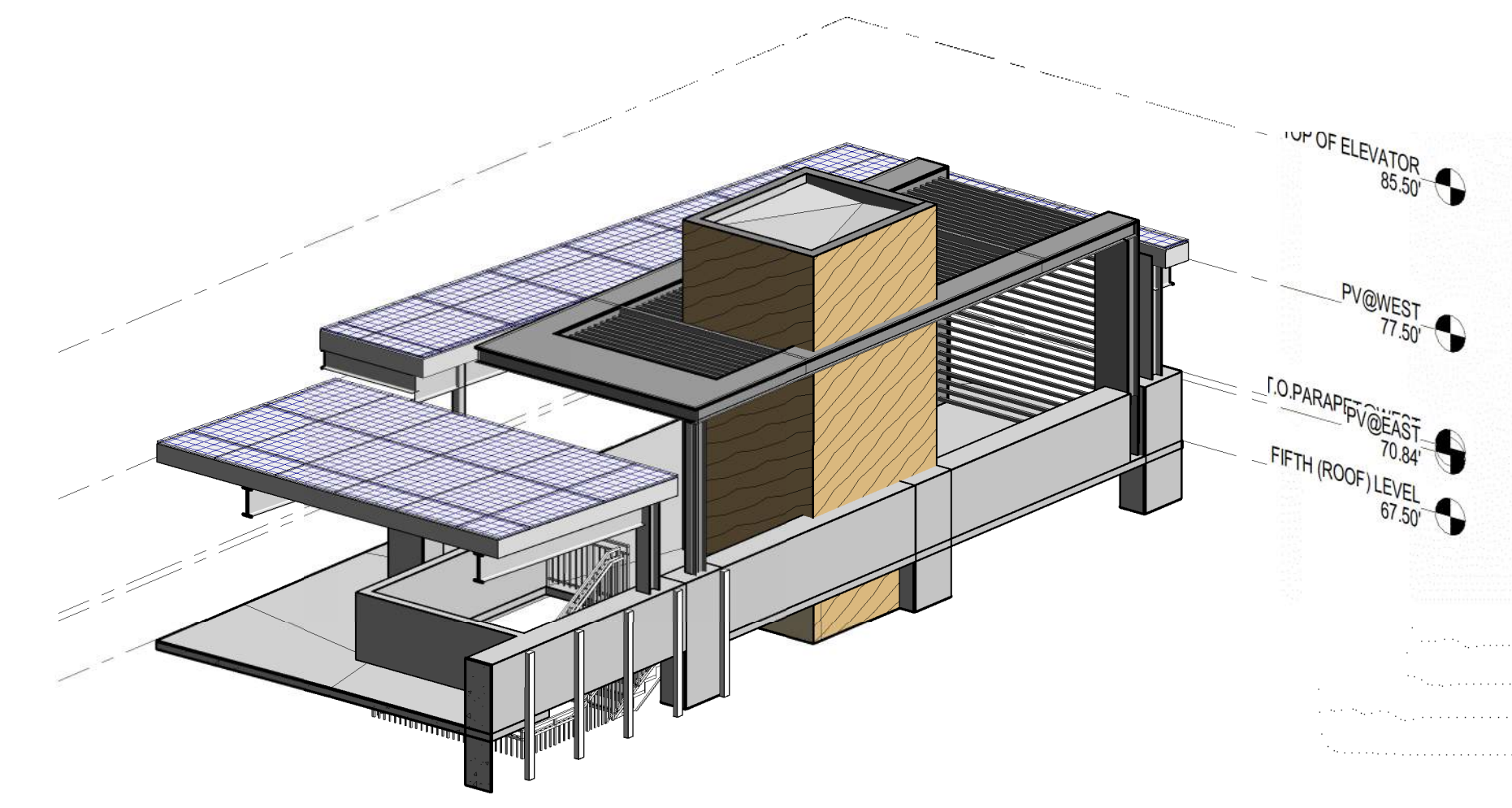
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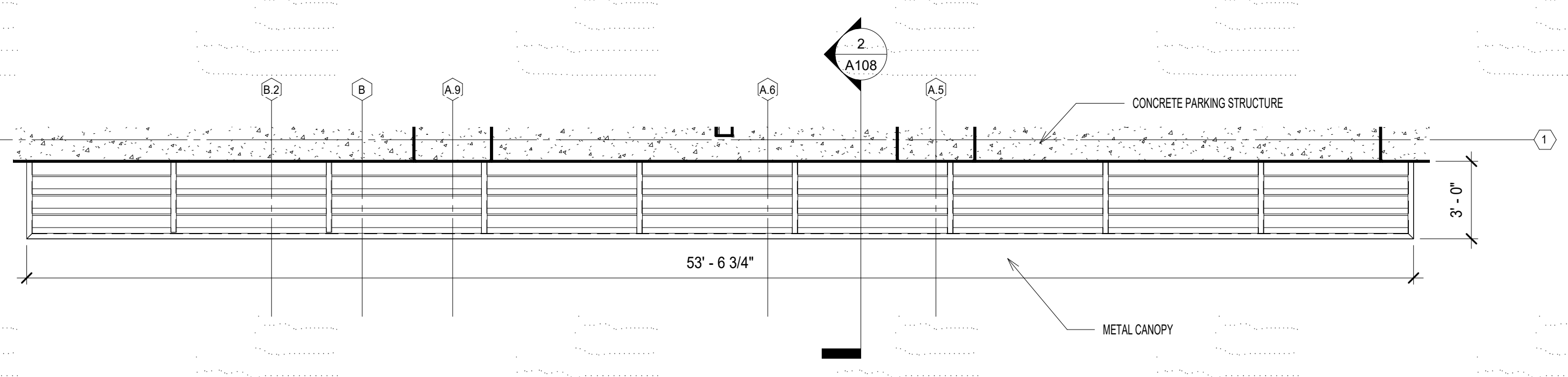
5 ROOF CANOPY SECTION
SCALE: 1/4" = 1'-0"



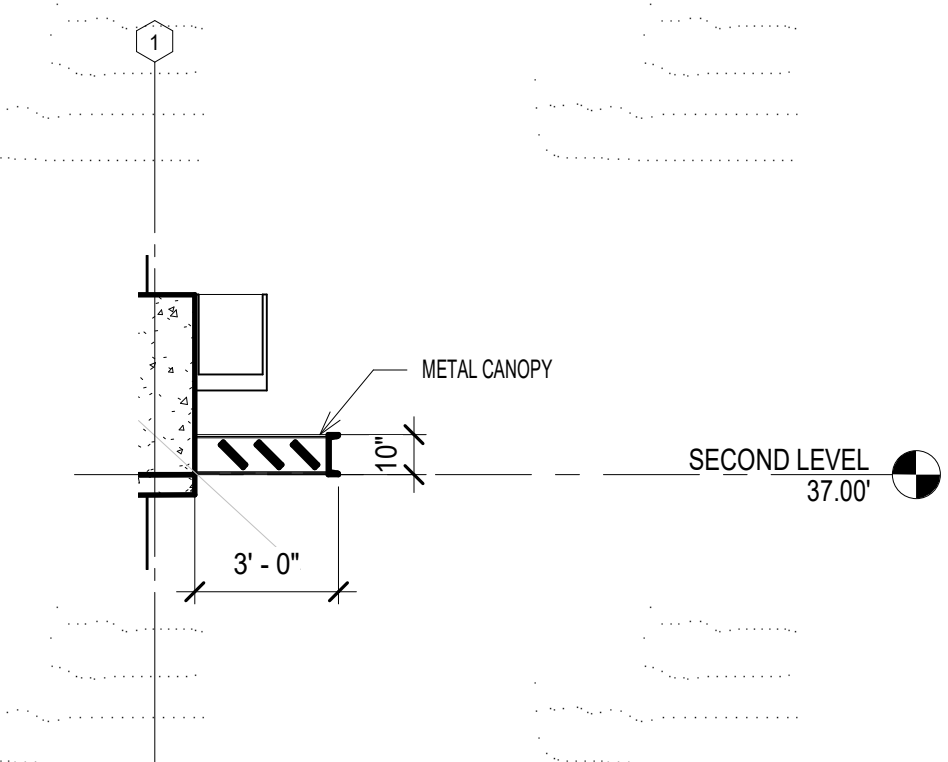
6 ROOF CANOPY PLAN
SCALE: 1/4" = 1'-0"



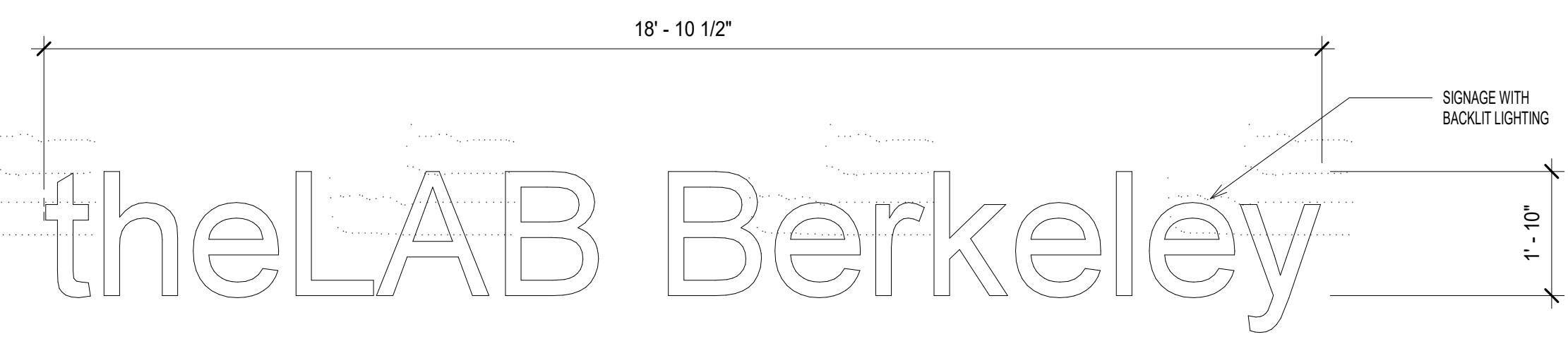
1 3D VIEW (ROOF TRELLIS)
SCALE:



3 CANOPY PLAN
SCALE: 1/4" = 1'-0"



4 CANOPY SECTION
SCALE: 1/4" = 1'-0"



7 ENLARGED SIGNAGE
SCALE: 1/2" = 1'-0"

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SITE PHOTOS | A110



1



2



3




4



5



KEY PLAN

SCALE: N.T.S. 

03/26/2021

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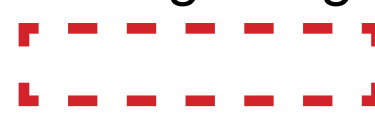
SHADOW ANALYSIS @ VERNAL EQUINOX | A111

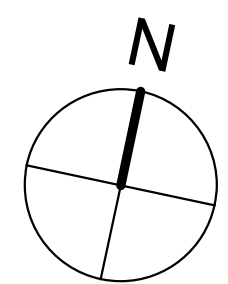


Note:
 *Shadow study has been accurately completed by computer-generating shadow analysis based on UTC (GMT) -8 h.

*All buildings being shadowed are shown on the diagram.

*Incremental shadow due to the proposed project is highlighted in color of 

*Existing building is highlighted in 



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SHADOW ANALYSIS @ SUMMER SOLSTICE | A112

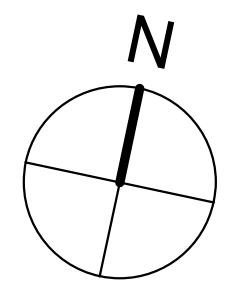


Note:
 *Shadow study has been accurately completed by computer-generating shadow analysis based on UTC (GMT) -8 h.

*All buildings being shadowed are shown on the diagram.

*Incremental shadow due to the proposed project is highlighted in color of

*Existing building is highlighted in





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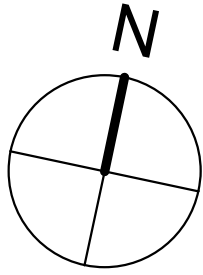


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SHADOW ANALYSIS @ WINTER SOLSTICE | A113

	EXISTING	PROPOSED	DIFFERENCE (Existing & Proposed)	
December 21, 2 hours after sunrise				December 21, 2 hours after sunrise
December 21, Noon				December 21, Noon
December 21, 2 hours before sunset				December 21, 2 hours before sunset

Note:
 *Shadow study has been accurately completed by computer-generating shadow analysis based on UTC (GMT) -8 h.
 *All buildings being shadowed are shown on the diagram.
 *Incremental shadow due to the proposed project is highlighted in color of 
 *Existing building is highlighted in 



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VIEW ALONG THE FOURTH STREET



VIEW ALONG THE FIFTH STREET

12/22/2021

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EXISTING AERIAL VIEW FROM NORTHWEST



EXISTING AERIAL VIEW FROM NORTHEAST



PROPOSED AERIAL VIEW FROM NORTHWEST



PROPOSED AERIAL VIEW FROM NORTHEAST

12/22/2021

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EXISTING AERIAL VIEW FROM SOUTHWEST



EXISTING AERIAL VIEW FROM SOUTHEAST



PROPOSED AERIAL VIEW FROM SOUTHWEST



PROPOSED AERIAL VIEW FROM SOUTHEAST

12/22/2021

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PHOTO SIMULATIONS - STREET VIEW | A117



EXISTING VIEW ALONG THE FOURTH STREET - FROM NORTH



EXISTING VIEW ALONG THE FOURTH STREET - FROM SOUTH



PROPOSED VIEW ALONG THE FOURTH STREET - FROM NORTH



PROPOSED VIEW ALONG THE FOURTH STREET - FROM SOUTH

12/22/2021

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PHOTO SIMULATIONS - STREET VIEW | A118



EXISTING VIEW ALONG THE FIFTH STREET - FROM NORTH



EXISTING VIEW ALONG THE FIFTH STREET - FROM SOUTH



PROPOSED VIEW ALONG THE FIFTH STREET - FROM NORTH



PROPOSED VIEW ALONG THE FIFTH STREET - FROM SOUTH

12/22/2021

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